

# TOWN OF VAIL

## STREETSCAPE MASTER PLAN

November 20, 1991

*Prepared by:*

WINSTON ASSOCIATES, INC.

## **ACKNOWLEDGEMENTS**

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This Streetscape Master Plan would not have been possible without the cooperation and involvement of the citizens of the Town of Vail.

This Master Plan was prepared under the direction of the Town of Vail.

### **Town Council**

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Thomas Steinberg, Mayor ProTem  
Lynn Fritzen  
Jim Gibson  
Merv Liptin  
Robert LeVine  
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Ronald Phillips

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Shelly Melio, Town Planner

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### **Arts in Public Places Board**

Patricia Story, Chairperson  
Kathy Langenwalter  
Bobby McIlroy  
Elizabeth Scott  
Dr. Tom Steinberg  
Jim Cotter  
Laura Nash

This Master Plan was prepared by:

### **Winston Associates, Inc.**

Paul Kubin, Project Manager  
Jeffrey Winston, Principal  
Dwila Davidson, Graphics and Drafting

Leslie Parchman, Drafting and Graphics  
Norma Egbert, Office Administrator

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## INTRODUCTION

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The ambiance of Vail is often cited as one of the Town's most valuable assets. Distinctive architecture, dramatic views and an extensive pedestrian environment give Vail a charm that is unique among resort communities. This Master Plan will focus on one aspect of Vail's physical environment -- the streetscape -- the public spaces among the buildings. The design of streets, plazas, site furnishings, lighting, and landscaping is as important as the design of individual buildings. The Vail Streetscape Master Plan will give the pedestrian environment the same comprehensive design and quality of materials that the Town's architectural standards have achieved. It will provide the Town with the tools to ensure the streetscape is consistent with Vail's quality design and architecture.

The Streetscape Master Plan will be used together with the Vail Village and Lionshead Urban Design Guide Plans, The Vail Village Master Plan, The Recreational Trails Plan, The Signage Improvement Program, and other adopted long-range planning documents. The guidance provided by these Plans will enhance the Town's position as one of the world's premier resorts.

### Purpose of the Streetscape Plan

The Streetscape Master Plan is an outgrowth of the Vail Village Urban Design Guide Plan (UDGP). The UDGP was created in 1982 to give guidance to the overall physical development of Vail Village. In addition to providing broad design guidelines, the UDGP suggested specific physical improvements for the Village. These suggested improvements included: the upgrading of several plazas, new landscape areas, building additions, facade improvements, etc. The development community, by working with Town Boards and Town staff, has included most of the designated improvements in development (or redevelopment) projects throughout the Town. *As a result, over the last 9 years more than 90% of the designated public/private improvements for Vail Village have been accomplished.* The Town has completed many of the recommended public projects and, furthermore, when private improvements have been made, the private property owners have often improved the public right-of-way adjacent to their property. These private improvements have included replacing deteriorating asphalt or concrete paving with decorative paving, creating new planters, installing light fixtures, etc. The results have generally been positive. The Streetscape Plan has been written in part to provide clear design direction for these types of improvements. The community has been actively involved in refining this plan to a point where there is general consensus on its scope and design themes. **The purpose of the plan is to provide a comprehensive and coordinated conceptual design for streetscape improvement that: 1) is supported by the community; 2) enriches the aesthetic appearance of the Town; and 3) emphasizes the importance of craftsmanship and creative design in order to create an excellent pedestrian experience.**

The Streetscape Master Plan also takes into account improvements to the Town's infrastructure. Water and sewer lines, drainage and transportation impacts need to be considered as well as aesthetic improvements when trying to improve public spaces. As an example, street surfaces can be replaced with materials and design themes that are consistent with the Master Plan when the existing street paving is removed to upgrade buried utility lines.

The Streetscape Master Plan will also address several other objectives which have been identified by the Town:

- 1) To provide a conceptual design for streetscape improvements that can be used for:
  - phased implementation of the plan sponsored by the Town - the proposed improvements are in no way intended to all be constructed at the same time because of costs and the desire to adapt to changing circumstances in the community;
  - privately sponsored improvements on public land associated with an adjacent building redevelopment;
  - establishment of a special development district;
  - joint public/private sponsored projects.
  
- 2) To establish a high level of quality and an overall character that is sensitive to the community's alpine setting for:
  - sidewalks
  - benches
  - lighting
  - signage
  - landscaping
  - street paving
  - special events or performance spots
  - walls
  - newspapers dispensers
  - utility pedestals and covers
  - bus stops
  - curbs
  - drainage improvements

The intent is to encourage craftsmanship and ornamentation that will create diversity and uniqueness while respecting the overall design fabric or character of the community.

- 3) To design improvements with a sensitivity to the contribution excellent landscape architecture can make to creating beautiful pedestrian spaces.
- 4) To design public spaces in a manner that encourages people to interact with each other and/or our mountain environment, whether it be for a special event, performance site, seating area, or a pedestrian path along Gore Creek.
- 5) To create interesting pedestrian ways (not a suburban grid) with undulating sidewalks that add interest and create opportunities for landscaping. Achieving this goal may require acquiring additional right-of-way or easements in some areas.

- 6) To accent key intersections and focal points with paving or other features such as public art, seating, landscaping or fountains to create more people places. There was strong support voiced during the public review process for more art and fountains in Vail Village.
- 7) To resolve conflicts between pedestrians and vehicles where they share the public right-of-way, (including creating safe pedestrian areas, increasing the efficiency of the bus system and providing access for emergency vehicles.)
- 8) To encourage the use of durable, high quality materials that reduce long-term maintenance costs and to develop low maintenance designs and to encourage the highest quality maintenance standards possible. Good quality maintenance and materials are essential to streetscape improvements being effective and useful year-round.
- 9) To propose a conceptual lighting plan that in subtle ways highlights buildings and landscaping to create an inviting evening pedestrian experience.

## The Process

The citizens of Vail have been extensively involved in the development of the Streetscape Master Plan. Many public meetings were held, during which residents, property owners and merchants gave valuable direction to the Plan. These included:

- Individual sub-area meetings with merchants, property owners and interested citizens at two points during the Master Plan process. The Village Core, Vail Valley Drive, West Meadow Drive/East Lionshead Circle and East Meadow Drive were all addressed as distinct areas at these meetings.
- Town-wide public meetings, one of which was a joint DRB/PEO/Town Council meeting
- Reviews by the Design Review Board, the Planning and Environmental Commission, the Transportation Committee and the Art in Public Places Committee

In the end, the effectiveness of this Master Plan has been greatly enhanced by this input. The Master Plan process also relied on the direct involvement of the Town of Vail staff. Input was also provided by other consultants who were working for the Town on related projects and each of the major local utility companies.



## **Design Process for the Master Plan**

The first step of the Master Plan process was a detailed inventory of the existing streetscapes within the study area. The purpose of this inventory was to identify the unique, and consistent aspects of the Streetscape environment. This inventory examined elements such as paving materials, site furnishings as well as general character. Another task in this phase was identifying circulation patterns of pedestrians and vehicles (cars, buses, delivery vehicles, emergency vehicles).

With the completion of the Inventory/Analysis phase, several preliminary design solutions were developed to address the problems identified. Conceptual designs for each study area were developed. These concepts were reviewed by the public and Town staff and refined several times before being formulated into specific Streetscape alternatives. After further review by the staff and public, preferred design alternatives were selected. Site specific details for key areas were also developed, along with guidelines for material selection and cost estimates.

## **The Study Area**

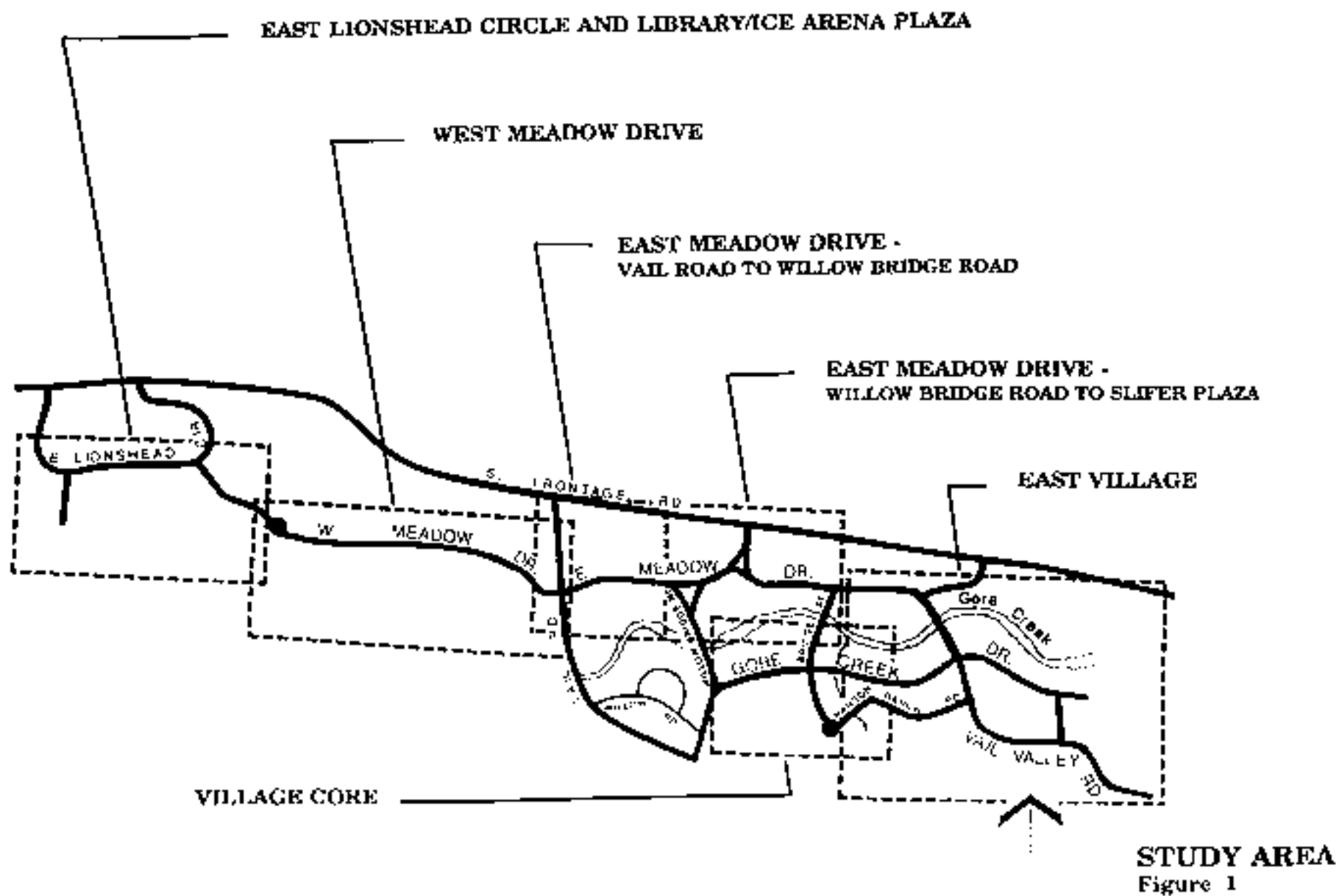
The Streetscape Improvement Master Plan study area extends from the east end of the Lionshead Mall to Ford Park (Figure 1). It is hoped that the Lionshead area will be addressed in a similar design process, currently scheduled for 1992 or 1993. The study area has been further divided into the following "sub-areas".

- East Lionshead Circle - East Lionshead entry to the Ice Arena/Library Plaza.
- West Meadow Drive - Ice Arena/Library Plaza to Vail Road
- East Meadow Drive - Vail Road to Willow Bridge Road (including Vail Road East Meadow Drive to the Frontage Road)
- East Meadow Drive - Willow Bridge Road to Vail Valley Drive (including Village Center Road and Slifer Square)
- The Village Core - Bridge Street, Gore Creek Drive, Mill Creek Court, Gore Creek Promenade, and portions of Willow Bridge and Hanson Ranch Roads
- The East Village Area - Vail Valley Drive (Blue Cow Chute to the soccer field), Hanson Ranch Road (Mill Creek to Vail Valley Drive) East Gore Creek Drive and East Meadow Drive - Slifer Square to Vail Valley Drive.

## Organization of the Report

The recommendations of the Master Plan are summarized in the sections that follow. Large scale versions of the Master Plan graphics are on file with the Town in the Community Development Department. The most significant aspects of the Plan are described in the text and additional design concepts and details can be found in the design drawings and associated notes. To make it possible to quickly review and compare each sub-area, a summary of findings, in a common format, has been used. Although this Plan was developed comprehensively, each sub-area will be addressed as a unit from inventory/analysis through final recommendations. It is important to emphasize that the report and drawings outline conceptual design ideas. Final design work is still necessary to refine these ideas so that important design and construction details are addressed. The format for each section is outlined as follows:

- Existing Conditions
- Character
- Circulation - Vehicular/Pedestrian
- Paving/Drainage
- Site Amenities
- Landscaping
- Utilities/Lighting
- Streetscape Improvements
- Preliminary Concepts
- The preferred streetscape plan - pedestrian circulation/landscaping and site amenities
- Costs



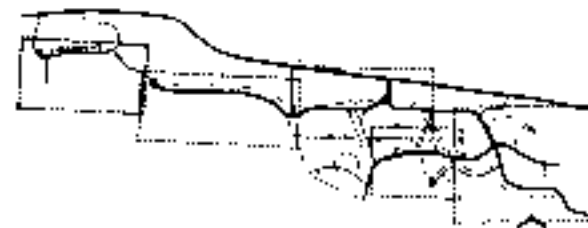
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## EAST LIONSHEAD CIRCLE AND LIBRARY/ICE ARENA PLAZA

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### Existing Conditions

This sub-area is composed of two distinct zones -- East Lionshead Circle and The Library/Ice Arena plaza. Because of the difference in character, each area will be covered separately within this section of the report. A graphic summary of the inventory/analysis of this area is shown in Figure 2.



### EAST LIONSHEAD CIRCLE

#### CHARACTER

East Lionshead Circle is the main portal to the Lionshead Mall. It has a more urban, contemporary character that differs markedly from the other parts of Vail. This is partly due to the height and mass of the surrounding buildings, the linear nature of the street and the presence of the typical curb/gutter/sidewalk combination (as compared to the Village where there are no curbs or sidewalks). It is very important that the final streetscape design for this sub-area create a strong pedestrian connection between Lionshead and Vail Village.

#### CIRCULATION

##### *Vehicular*

There are currently no restrictions to vehicular traffic in this area. Buses, cars, service vehicles and bicycles all share the roadway. There are several driveway cuts along the south side that access lodge units.

##### *Pedestrian*

There are major pedestrian flows both in and out of the parking structure portals. At the west end of the parking structure, the pedestrian movement is primarily in two

directions; southwest to the Lionshead entry, and south to the Treetops commercial area (see Figure 2). Pedestrians have actually worn a path through one of the planting medians in the bus stop area in order to get from the drop-off to the bus stop. At the east end of the parking structure, the major pedestrian movement is southeasterly to the Library/Ice Arena plaza.

Existing pedestrian flow along East Lionshead Circle appears to favor the striped pedestrian path on the south side of the street, although both sides of the street are used by pedestrians.

Because of heavy vehicular traffic in this area, it is desirable to separate pedestrian and vehicular traffic. The width of the road right-of-way appears adequate on East Lionshead Circle to accommodate an increase in the area dedicated to the pedestrian on one or both sides of the road.

## PAVING/DRAINAGE

The walkways and pedestrian areas are generally in good condition.

- The existing 6' wide concrete sidewalk along the north side of the street appears to be in good condition, (though the walk is often not plowed in the winter.)
- Concrete unit pavers are used at the east and west portals of the parking structure, in crosswalks, in the East Lionshead entry plaza and in front of the Treetops retail shops at the bus turnaround.
- The roadway is asphalt with a striped pedestrian path on the south side.

Drainage improvements are needed along East Lionshead Circle. All storm water flows are to the south to Gore Creek in this area. According to the Muller Engineering Drainage Report, this pattern will be maintained. This will require that the driveway entrances along this south side of the street be elevated above the anticipated flow line at the curb to interrupt the storm water.

## SITE AMENITIES

This sub-area has many public and private artworks. At the west portal of the parking structure, Kaijoo III, Bird of Paradise and "The Lift" (the skier sculpture) can be seen along with a changing display of public and private artwork sponsored by the local galleries. At the east portal of the Lionshead Parking Structure, Clip Man,

sponsored by the Vail Valley Arts Council is located. "The Need to Know" is located on the south side of the Library.

There are trash receptacles and newspaper dispensers at the west entries to the parking structure and in the Lionshead Mall east entry plaza. There are no benches, bike racks or trash receptacles, in the rest of the area. Additional site furnishings along the north side of the street, and at the east end of the sub-area, would improve the street environment.

## LANDSCAPING

The landscape character of East Lionshead Circle is dominated by the sloping lawn immediately south of the parking structure. This landscaped area gives the corridor an open feeling. There are a number of 10-15 year old trees that give the lawn area a mature landscaped appearance. Landscaping for the lodging units along the south side of the street provides screening for their parking and also softens the street edge. On the west end of the street, at the bus drop-off, the landscaping is confined to planters - presenting a more urban character.

## UTILITIES

There are utility transformers and pedestals at the edge of the right-of-way in the area. They are primarily on the north side of the street, in the lawn area next to the parking structure.

### *Lighting*

The lighting level in the East Lionshead Circle area is higher than is typically found in other parts of the Town; however, there are still areas where the light levels are inadequate. The contemporary Lionshead light fixtures are used throughout the area. A decision will need to be made as to how, and where, to make a transition from the Lionshead fixture to the Victorian "Village" fixture found further east. Consideration also should be given to finding a new light fixture that is more compatible with Lionshead.

There is a wide variety of light fixtures now in use in the Library/Ice Arena Plaza. A design theme for lighting should be selected to avoid having so many different lights.

## LIBRARY/ICE ARENA PLAZA

### CHARACTER

The enclosing landforms and tall, mature trees give this area a strong sense of being an outdoor room. Significant streetscape improvements for this space were completed

nine years ago with the addition of rock walls and concrete unit pavers. A goal for the Master Plan will be to extend the streetscape character of the Library/Ice Arena Plaza to East Lionshead Circle and West Meadow Drive.

## CIRCULATION

### *Vehicular*

Traffic in the Library/Ice Arena Plaza is restricted to Town of Vail buses, emergency vehicles and service and delivery vehicles. Control gates are provided at each end.

### *Pedestrian*

The short roadway often called the "Chute" that connects East Lionshead Circle to the plaza is the primary area of concern. Private cars are restricted from this area, however, this section of street is much too narrow to adequately handle the volume of pedestrians and buses that must share the roadway. The roadway should be widened or a separate walkway provided for pedestrians.

If a new walkway is added, the south side of the road should be used since there are retaining walls on the north side. Existing, large trees and utility pedestals on the south side will need to be avoided when designing a parallel pedestrian path.

The plaza functions very well as a pedestrian zone for the most part. One area of concern relates to the steepness of the "Chute" leading into the plaza. Both bicycles and rollerbladers are often traveling at unsafe speeds at the bottom of the chute where it enters the plaza. A means of slowing down these two user groups will need to be explored. Consideration could be given to making this area (from control gate to control gate) a bike dismount zone. Further study will be necessary to analyze the conflicting uses in this confined and heavily used area.

## PAVING/DRAINAGE

Concrete unit pavers are currently used in the Library and the Ice Arena Plaza. There have been some settling problems due to buses using portions of the plaza that were not originally designed for bus traffic. In general, the paving system is working well and does not need to be replaced or redesigned.

Special paving treatments for the "chute" may be appropriate to direct pedestrians off the road and to slow down bikes and rollerblades.

The Muller Engineering Drainage Report has recommended \$15,000 of drainage improvements in this area. The Library/Ice Arena Plaza is lower than the surrounding area, and this results in storm water collecting in the plaza. Depending on the final design and locations for these improvements, it may be possible to upgrade the

carrying capacity of the existing concrete unit pavers to better accommodate bus traffic when the new storm drains are installed.

## **SITE AMENITIES**

There are trash receptacles at the building entries, picnic tables and several rock walls that are at seating height. However, there is a need for more seating in the plaza area. There are bike racks near the library and ice arena entries but more are needed.

## **LANDSCAPING**

The presence of very large, mature spruce throughout the Library/Ice Arena Plaza gives this area a strong overhead canopy. There are few other portions of the study area where tree canopies of this type can be found. As a result, there is little room or need for additional landscaping.

## **UTILITIES**

Several above-ground utility pedestals need to be avoided if a parallel pedestrian path is constructed next to the Library Chute.

### *Lighting*

Lighting levels throughout the Library/Ice Arena Plaza are too low. Consideration should be given to using landscape lighting to illuminate the pedestrian plaza and adjacent sidewalks. Consideration should be given to snow depths and winter-time lighting.

## **PUBLIC COMMENT**

The comments that were received focussed on:

- The problems of bicycles and rollerbladers entering the library plaza at high speed
- The need for a separate pedestrian path next to the Library Chute
- Recognize and allow for future development next to the parking structure, (i.e., performing arts center)
- The importance of separating pedestrians and vehicles on East Lionshead Circle
- A sidewalk is needed along the west side of Lionshead Circle from the bus drop off to the Frontage Road





## Streetscape Improvements

### East Lionshead Circle - Library/Ice Arena Plaza

The improvements suggested in the Streetscape Master Plan for this westernmost sub-area emphasize safe pedestrian movement and aesthetic improvements to pedestrian areas between Lionshead and West Meadow Drive. Most of the suggested changes are targeted at the "Chute," the short, steep road section that connects East Lionshead Circle to the Library/Ice Arena plaza. The remaining improvements either formalize existing pedestrian patterns or add site amenities.

#### PRELIMINARY CONCEPTS



Some of the preliminary ideas or concepts considered included the following:

- Add a sidewalk on the south side of East Lionshead Circle (at street level and not separated with a curb). Not having a curb would make the walkway easier to maintain, however, it was felt that it did not create a safe pedestrian environment unless further efforts were made to reduce vehicular traffic.
- Provide a pedestrian path parallel to the Library Chute. Because a straight walkway would allow rollerblade users to build up too much speed before entering the plaza, a pronounced jog was suggested.
- Pave the Library Chute with concrete unit pavers. This idea was later scaled back to reduce the emphasis of this area as a pedestrian path.

#### PREFERRED STREETScape PLAN

*Design Concept:* The primary design concept is to strengthen the landscaping in this area to bring back the feel of walking along the nearby Gore Creek corridor. A strong emphasis is placed on planted materials, artwork and ornamentation (benches, etc.) to create the visual interest and beauty that will make this area more attractive to the pedestrian. The landscape design should try to create the sense of enclosure and dramatic setting achieved in the Library/Ice Arena Plaza. Early photographs of this area indicate that the large evergreens along the creek corridor extended up into this area. The design concept draws upon this original

natural setting. The preferred Plan for the East Lionshead Circle and Library/Ice Arena Plaza sub-area is illustrated in Figure 3.

The preferred Streetscape Improvement Plan for East Lionshead Circle strengthens the desired pedestrian-oriented character. The proposed changes separate pedestrian and vehicular traffic by defining travel lanes with distinctive paving patterns and colors, rather than with a raised curb and grade separation.

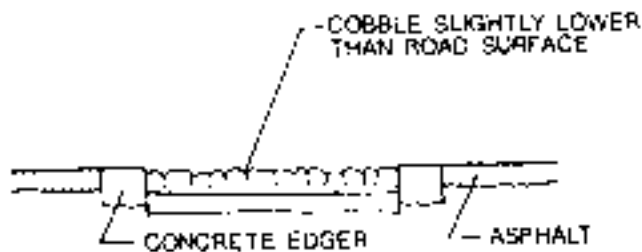
Generally, the existing vehicular circulation patterns will remain unchanged since they are functioning well, however, the road surface will be narrowed to a minimum width of 24' (curb-to-curb) to dedicate as much of the right-of-way as possible to the pedestrian. The Vail International property should be encouraged to relocate their vehicular access to the South Frontage Road, and the Lodge at Lionshead should be encouraged to consolidate their driveways along East Lionshead Circle. This would further reduce pedestrian/vehicular conflicts on East Lionshead Circle.

#### *Pedestrian Circulation*

The proposed Plan features the following changes to the pedestrian circulation system (Note: all new walkways are proposed to be constructed with concrete unit pavers):

- The control gate at the top of the Library/Ice Arena Plaza should be moved to the west, adjacent to the east side of the Treetops Commercial Building. This would further enhance the pedestrian experience along East Lionshead Circle. However, the control gate should not be relocated unless the vehicular access improvements, suggested for the Vail International and Lodge at Lionshead projects, are completed. Additionally, an at-grade (flush) 6' - 10' wide, concrete unit paver walkway is suggested for the south and/or north sides of the street from the relocated control gate to the Library Chute. The final size and location for these walkways will be based on results from further pedestrian traffic studies to be performed by the Town. The south walkway will cross a number of driveway curb cuts that will necessitate "warping" the surface to keep all drainage run-off in the public right-of-way. Stone or boulder retaining walls may be needed along the walk in front of the Treetops Commercial Center.

- A stronger east entry into the Lionshead Mall is proposed. The existing crosswalk would be widened as well as the walkway connecting to the stair in front of the Lionshead Center Building. Landscaping will be added. The intent is to create a sense of entry into the Lionshead Mall at this point. This will be a challenge as the pedestrian pattern tends to show that people want to walk through the bus turnaround which conflicts with bus traffic. A stronger landscape treatment in the planting areas is recommended to enhance the appearance of the entry and to direct pedestrians to the entry into Lionshead Mall.
- A new crosswalk from the west portal of the parking structure to the Treetops Commercial Center. The crosswalk is then linked to the raised walkway in front of the shops (some steps will be required to avoid making the walk too steep.) This is in response to an existing pedestrian pattern along that alignment.
- A crosswalk connecting the east portal of the parking structure to the new Library Chute pedestrian path. This crosswalk branches off just before the bus control gate and makes the connection to the west door of the ice arena. The proposed improvements on the north side of East Lionshead Circle will need to be reexamined for compatibility, as the details of the design of the proposed performing arts center are completed.



- Add perpendicular bands of rough textured pavement (cobble or a similar material) to the asphalt roadway of the Library Chute. The bands should be slightly lower than the roadway to avoid damage from snow plows. The control gate and adjacent totems should be configured to discourage access for pedestrians and rollerblades, while still providing adequate operating area at the gate for buses (2'-0" or less between totems). The intent is to discourage pedestrians and rollerblades from using the Chute and to direct all non-motorized traffic to the new sidewalk on the south side of the Chute.

The Chute and adjacent path, the Library/Ice Arena Plaza and the roadway connecting the plaza to West Meadow Drive could be designated as bicycle dismount zones. While there was concern that dismount zones are difficult to enforce, it was felt that there were good reasons for trying the concept in this area if traffic conflicts persist.

- Replace the existing 6' wide concrete sidewalk on the north side of East Lionshead Circle with a 6' to 10' wide, concrete unit paver walk. Widening the walk may require relocating some light fixtures and utility pedestals. The walk should undulate slightly with strong landscaping along the walk to add pedestrian interest.

Since this side of the street is already served by an existing walk, replacing it with a wider path is a low priority. However, if the goal of keeping the pedestrians out of the roadway for safety concerns is to be met, the wider walk will be necessary.

- A new 8' wide undulating walkway adjacent to the Library Chute roadway. Because this area is in shade much of the time, consideration should be given to heating the walkway. The walk will need to avoid the utility transformer. The final design of the walkway must consider the possibility that rollerbladers, pedestrians, and bikers may use the walkway at the same time. Acquiring additional right-of-way is essential for this pedestrian connection.
- A 6' (minimum) concrete unit paver walk is proposed at the west end of East Lionshead Circle, on the west side of the street, to make a pedestrian connection to the Frontage Road. This walk should avoid the existing landscaping as much as possible. The roadway could be narrowed to a 24' width if necessary.
- If a sidewalk is installed along the South Frontage Road, a walkway will be needed at the west end of the parking structure, from the west portal to the Frontage Road.

The existing concrete unit paver crosswalks and pedestrian plazas at the west and east portals of the Lionshead parking structure and the East Entry to Lionshead Mall will remain unchanged.

No physical changes are proposed for the Library/Ice Arena Plaza, only site amenities will be added. Picnic tables should be added along the existing walkway on the north and east sides of the Ice Arena.

## *Landscaping and Site Amenities*

Additional landscaping is shown in, or adjacent to, the right-of-way throughout this sub-area. In some cases, its purpose is to screen adjacent parking and in others, to soften the long, linear facade of the parking structure. Additional landscaping will help break up the linear character of East Lionshead Circle.

A south-facing seating area is proposed along the parking structure walk (Figure 4). An opportunity for public art or a fountain is also noted. This design effort could be pursued with the help of the Vail Valley Arts Council. The "Art Walk" concept could also be integrated with the "Art Walk" along East Meadow Drive which would help to connect the Village and Lionshead. As with all seating areas, trash receptacles and low level lighting should be provided as well as a change in pavement texture and/or color. This part of the walkway has great potential for a site specific artwork to be integrated into the landscape, seating and sidewalk design.

One to two additional bike racks and several seasonal benches should be added in the Library/Ice Arena Plaza. Potential location for benches would be along the curved walls at the entry to both the Ice Arena and the Library.

Landscaping should also be added to the south-facing hillside, at the east end of the Lionshead parking structure, depending on the location of the proposed Performing Arts/Conference Center. Planting along the west end of the parking structure is also needed. Snow storage needs must also be considered.

Additional landscaping along the banks of Sandstone Creek is proposed to bring back the natural character of the creek area and to screen views of the adjacent hospital parking from the recreational path.

## COSTS

The following cost estimate is based on the above improvements. No allowance has been made for drainage improvements which have been proposed in a separate Master Drainage Plan by Muller Engineering. It is important to keep in mind that these costs are developed to a Master Plan level and will change as the project evolves during the detailed design process.

Demo asphalt/concrete	11,700 sf x \$4.50/sf	\$52,600
Demo softsurface	11,350 sf x \$1.25/sf	\$14,200
Patch asphalt	1,240 sf x \$2.20/sf	\$2,800
Curb and gutter	620 sf x \$13.00/sf	\$8,100
Concrete unit pavers	19,000 sf x \$6.00/sf	\$114,000
Lighting	4 x \$2,500	\$10,000
Benches	5 x \$600	\$3,000
Trash receptacles	2 x \$450	\$900
Newspaper dispenser (6 per cluster)	2 x \$2,500	\$5,000
Deciduous trees (inc. irrigation)	100 x \$300	\$30,000
Coniferous trees (inc. irrigation)	80 x \$350	\$28,000
Shrub beds and irrigation	3,000 x \$3.00	\$9,000
Signage	Lump Sum	\$2,500
Subtotal		\$280,100
Relocate Utilities	10%	\$28,010
		\$308,110
Miscellaneous Contingency	15%	\$46,216
		\$354,326
Design & construction management	20%	\$70,865
<b>Total</b>		<b>\$425,191</b>

Note: It is essential that maintenance budgets be increased, especially snow plowing, to cover the cost of adequately maintaining the new streetscape improvements.

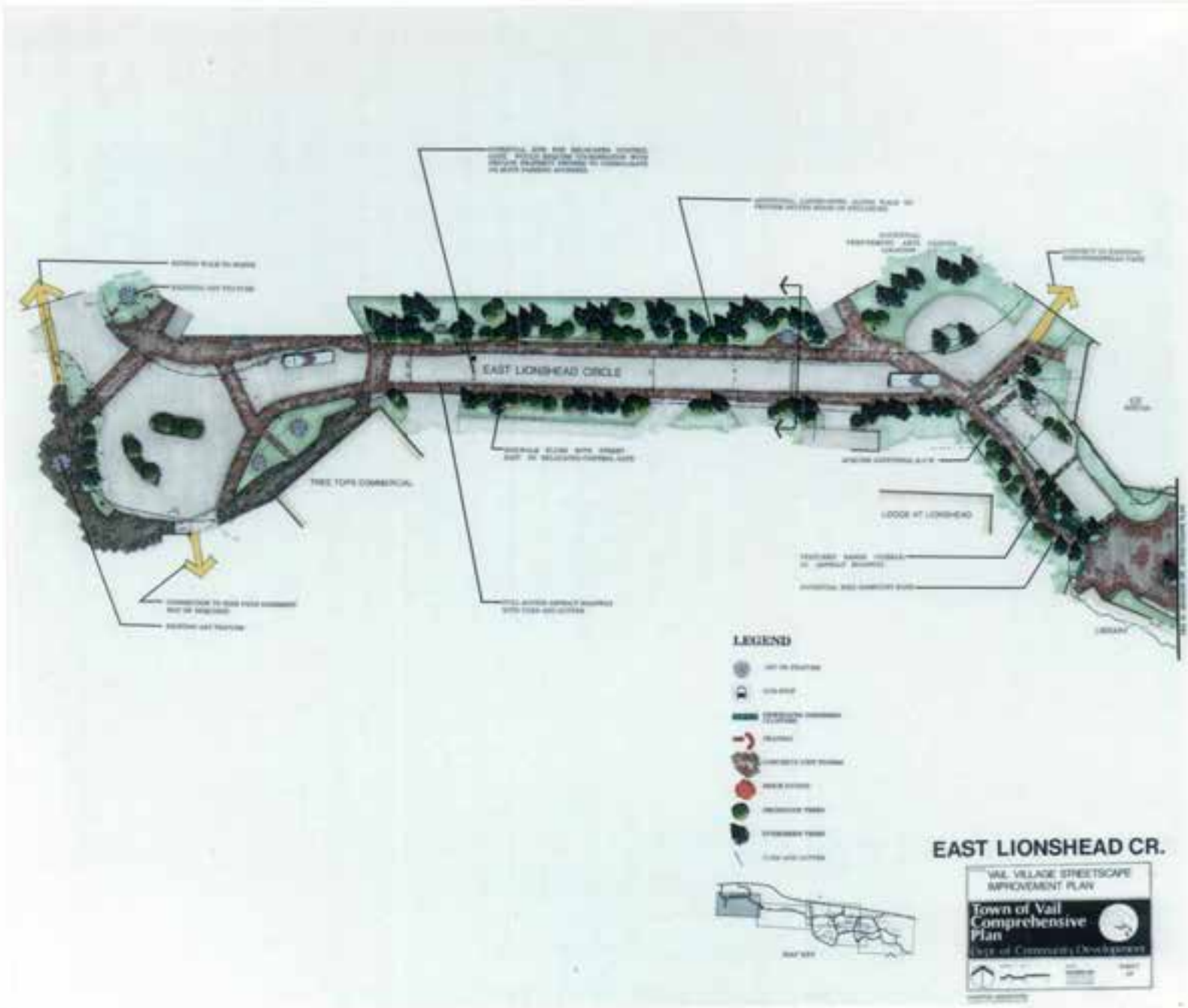
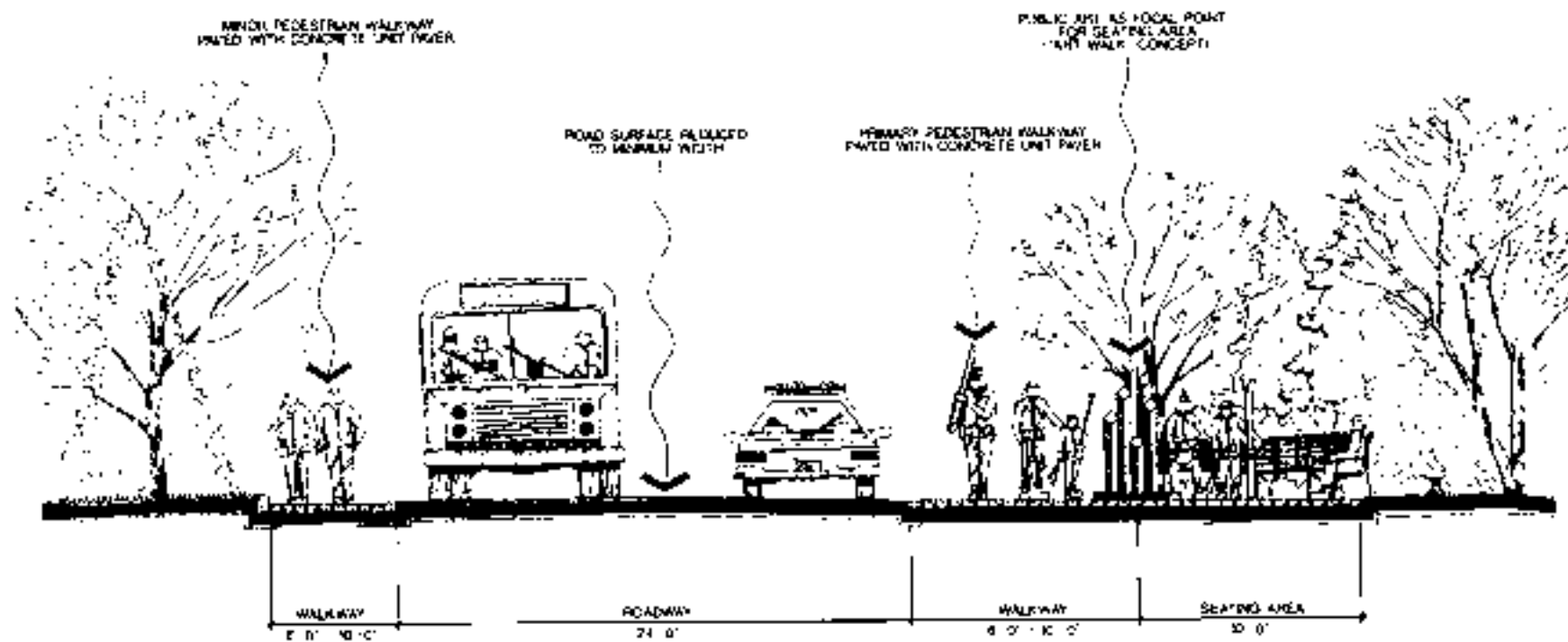


Figure 3





SECTION A  
EAST LIONSHEAD CIRCLE

Figure 4

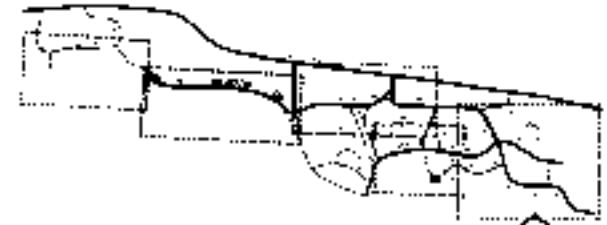
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## WEST MEADOW DRIVE

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### Existing Conditions

The West Meadow Drive sub-area includes all of the area from Vail Road to the cul-de-sac at the west end of West Meadow Drive and the roadway that connects the Library/Ice Arena Plaza to West Meadow Drive. The Inventory and Analysis graphic for this area is summarized in Figure 5.



### CHARACTER

West Meadow Drive is a long, gently curving street that has an open feel due to generous setbacks of the surrounding buildings. It is used by Town buses and private vehicular traffic. Because there is no curb and gutter or continuous sidewalk, pedestrians and vehicles share the street. This mixing of pedestrians and vehicles has raised safety questions. The problem is further complicated by the inclusion of emergency traffic (i.e. ambulances) for the hospital. The streetscape design for West Meadow Drive will require accommodating these sometimes incompatible uses.

West Meadow Drive's open character offers excellent background views of the ski mountain and provides a strong middle-ground visual connection to the large evergreen trees along Gore Creek. The foreground residences along the south side of the street gives the corridor a "neighborhood" quality.

The intersection of Vail Road and West Meadow Drive is one of the primary intersections in Vail. The potential to further pedestrianize this intersection has been strengthened by the Town's removal of the Ski Museum building and immediate plans to replace it with a pocket park. Removing the Ski Museum

building provides the space necessary to improve the intersection, and also creates a visual connection between East and West Meadow Drives forging a stronger link between Vail Village and Lionshead.

## CIRCULATION

### *Vehicular*



SHARED USE OF THE ROADWAY  
ON WEST MEADOW DRIVE

### *Pedestrian*

There are no restrictions on vehicular traffic on West Meadow Drive. Buses, cars, service vehicles, emergency vehicles and bicycles all use the same roadway. The variety of vehicles using West Meadow Drive, combined with the very heavy, on-street pedestrian traffic, reduces bus efficiency and raises concerns about pedestrian safety. Two bus stops, for east and west bound buses, at the mid-point of West Meadow Drive, further increase the congestion in this sub-area.

Because of the residential units (single and multi-family) located along both sides of West Meadow Drive, there are numerous private driveways connecting to the street. In addition, there are bays of head-in (perpendicular) parking spaces along both sides of the street. As it is not desirable to locate a major pedestrian way behind parked cars, this parking pattern will complicate the alignment of future pedestrian ways.

The wider street right-of-way appears adequate to accommodate a generous sidewalk and two lanes of traffic, with ample room left over for planting areas.

West Meadow Drive is heavily used by pedestrians. It is the primary pedestrian route between Vail Village and Lionshead Mall. Currently, most pedestrians walk in the street; however, a small number use the narrow (5' wide) concrete sidewalk on the north side bordering the hospital. There appears to be no preference by the pedestrians for one side of the street over the other, except at the east end, where most of the pedestrians cross Vail Road on the north side of the intersection.

West Meadow Drive is connected to the Library/Ice Arena Plaza to the west by a short segment of roadway with a control gate restricting access at the cul-de-sac. This roadway is heavily used by pedestrians, but because private vehicular access is restricted, few conflicts occur. An asphalt recreation path runs behind the library and connects West Meadow Drive to the Lionshead Mall.

## PAVING/DRAINAGE

The roadway throughout this sub-area is paved with asphalt. There is a short segment of 5' wide concrete walk next to the hospital. This sidewalk ends on both the west and east boundaries of the hospital property, and is elevated approximately two feet higher than the roadway. Because of this height difference, the narrow width of the walk, and the fact that most people enjoy walking in the street, the walk is not well used. There are no other public walks parallel to West Meadow Drive.

West Meadow Drive will require extensive storm drain improvements. Curb and gutter and storm inlets are needed along the length of the street. In addition, the elevations and slopes of the street surface will need to be adjusted to assure that storm water is directed to inlets. These improvements will likely necessitate the reconstruction of the street paving throughout the sub-area. (This cost has not been included as part of the Streetscape Improvement Plan.) The drainage improvements will also need to be coordinated with any pedestrian crossing recommended as part of the Master Plan.

## SITE AMENITIES

There are no seating areas along the entire length of this study area except at the bus stops. There are "wooden-barrel" trash receptacles at each bus stop. No bike racks are provided.

## LANDSCAPING

The landscape character of the south side of West Meadow Drive is softer than most areas in the Village. The residences have mature plantings of 10 to 20 year old evergreen and deciduous trees. Some of the largest spruce trees in Vail are located on the west end of the street near the library. These trees provide a strong enclosure for this area. The north side of the street seems harsher due to less mature plantings, the presence of larger structures and more perpendicular parking adjacent to the street. The most significant open space on the north side of West Meadow Drive is the large sloping lawn area next to the Holiday Inn.

## UTILITIES

The south side of West Meadow Drive is a major utility corridor. Numerous utility pedestals and transformers can be found along the entire length of the roadway. Several utility pedestals are also located on the north side of the street. Moving this equipment could be expensive. Any future improvements should attempt to

avoid the pedestals and transformers and screen them with landscaping as much as possible.

### *Lighting*

Regularly spaced, 35' tall "Town and Country" light fixtures are located along the south side of the street. A decision will need to be made whether the "Village" light fixture should replace the taller existing "Town and Country" fixture. The entry to the hospital is brightly lit with bollard lights. Only security flood lights and building entry lighting are found on the north side of the street. Lighting in this area is not adequate except around the hospital entry.

## **PUBLIC COMMENT**

Throughout the Master Plan process, West Meadow Drive was frequently mentioned as a primary area of public concern. In addition to the public meetings, individual reviews were held with property owners in this sub-area to gather their comments and ideas. The comments listed below are not exhaustive, but represent a cross-section of the concerns expressed about West Meadow Drive.

### Traffic:

Vehicular traffic was a primary concern. There was a strong need to reduce volume of vehicular traffic on this street. Most agreed that mixing the existing volume of vehicular and pedestrian traffic in the roadway was inappropriate because of safety concerns. In addition, there was strong direction to reduce the numbers of lost visitors who now travel down the entire length of the street before turning around. A card-activated control gate to restrict traffic was one idea mentioned to minimize traffic. Others wondered whether a gate system would be functional for guests staying in short-term lodge units, and if a control gate gives an unfriendly message to Vail's visitors.

### Lighting:

Many residents remarked that lighting levels along the road and at the intersection of Vail Road and West Meadow Drive were too low. Notwithstanding the need for additional pedestrian lighting, the Town must avoid overlighting the nearby residential units.

Separate  
Uses:

There was general agreement that the Master Plan for this area should recommend separating pedestrians, bikes and vehicles as much as possible because of safety concerns and a concern to allow for more efficient bus movements.

Art Walk:

Residents in this sub-area felt that the length of the street provided an excellent opportunity for displaying public art. The art pieces could be placed at key points such as bus stops and seating areas, creating an "Art Walk". If widely accepted, this idea could be extended to other areas in Vail, especially along East Lionshead Circle.

Durable  
Materials:

There were concerns expressed about cost, but all preferred concrete unit pavers to concrete or asphalt surfaces.

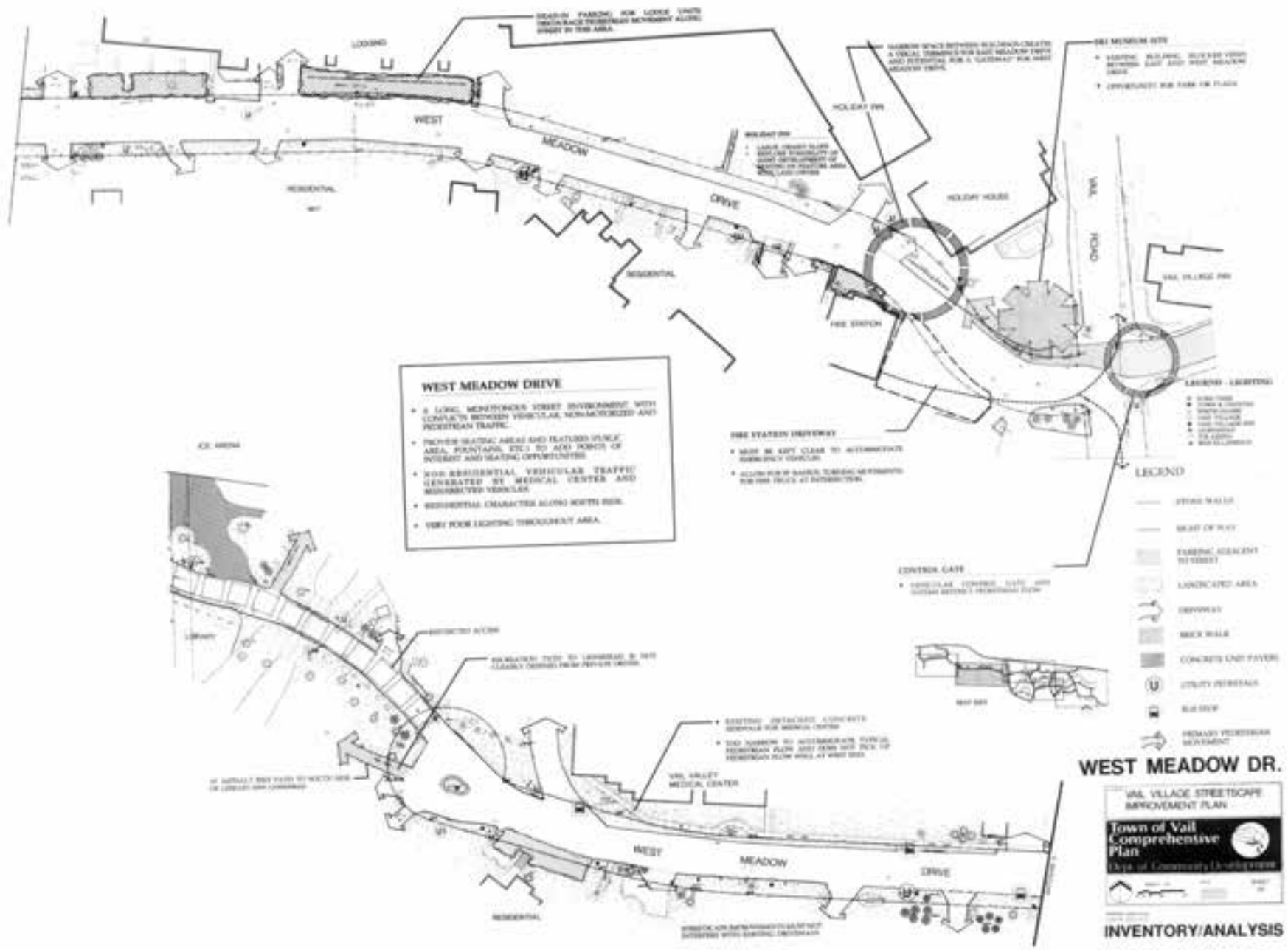


Figure 5

# Streetscape Improvement Plan

## *West Meadow Drive*

Public interest in the West Meadow Drive sub-area was strong throughout the design process. The adequate right-of-way allowed for the design of a wide range of streetscape options for the corridor. These concepts have been consolidated and refined into the proposed Streetscape Plan (Figure 6).

The Plan resulting from this extensive public involvement attempts to create a safe, clearly identifiable pedestrian corridor separated from the heavy vehicular traffic found on West Meadow Drive. Providing relief from the long, linear character of the street by the addition of significant amounts of landscaping is another goal, as is providing places for people to stop and rest.

### PRELIMINARY CONCEPTS

The preliminary concepts for West Meadow Drive focused on defining the existing pedestrian circulation patterns. This need to define the pedestrian circulation system led to the development of the following preliminary concepts.

- Use different paving treatments to create in-street pedestrian paths at the roadway level. This concept builds on the idea that part of the charm and fun of Vail is the ability to walk in the street. A street-level walk system is easier to maintain, but it was felt that the high volume of cars, buses and trucks using West Meadow Drive would create street-level pedestrian paths that would appear to create an even wider road without providing pedestrian safety.
  
- To construct sidewalks of equal width on both sides of the street. Since this is the pattern that most people are familiar with, it would be user friendly, but would result in relatively narrow walkways and increased pedestrians walking behind parked cars. In addition, it was felt that this system would do little to break up the monotony of the street.



Other ideas that were mentioned included:

- Use landscaped medians to soften the corridor and separate uses. This idea was quickly rejected as being "too suburban" for Vail.
- A manned check-point or a card-activated control gate at or near the intersection of Vail Road and West Meadow Drive to eliminate unnecessary vehicular traffic. This concept would only be possible if and when the hospital access completely switches to the South Frontage Road. In the end, this idea was rejected. It was considered to be too complicated to operate and would give the street an unfriendly, "elitist" character.
- The issue of bringing pedestrians along the fire station's driveway, and behind cars parked at lodge units along the street was debated extensively. Some felt both conditions were unsafe and should be discouraged while others believed that paths in these locations were necessary. The final plan does include minor walkways behind the parked cars to assure a continuous system, but it was felt that pedestrian traffic was inappropriate along the fire station driveway.

It was obvious from the first design study that all of the proposed options for the streetscape improvements would need to leave the existing vehicular circulation patterns intact. This was due to the number of private homes on the street and the need to maintain adequate access for Town buses and emergency vehicles.

## PREFERRED STREETSCAPE PLAN

The concept that received the broadest public support was to create a primary pedestrian path (10' - 12' wide) on one side of the street and a smaller sidewalk (5' wide) on the other. The primary pedestrian path crosses from the north to the south side and then back again, to avoid the head-in parking. Curb and gutter would be used to define the street which has been narrowed to the minimum width of 26', curb-to-curb.

### *Design Concept:*

Early on, it was recognized that, in Vail, people enjoy walking in the street. Because of safety concerns along West Meadow Drive, it became essential to create a separation between the pedestrians and vehicular traffic. The concept is to, in essence, create a new pedestrian street that is designed in such a way that it will be a more exciting pedestrian space than the existing asphalt street. The design calls for intense planting along the "pedestrian street" to bring back the landscape character which still exists immediately adjacent to Gore Creek. This approach will also carry the landscape design theme from West Meadow Drive through to East Lionshead Circle to strengthen the connection between Lionshead and the Village.

A second design concept is to add to the pedestrian experience by creating an "artwalk" along this corridor. This idea will also compliment the existing artwork located on East Lionshead Circle. The intent is to involve artists and the neighborhood residents in the final design of the "artwalk" to allow for site specific art pieces. The pedestrian street character is illustrated in Figure 7.

The final plan, illustrated in Figure 6, includes a number of unique features:

- All pedestrian paths be constructed with concrete unit pavers to clearly distinguish them from the roadway. The primary pedestrian path may be a different color and/or style of unit paver than the minor walkway.

Having a walkway width of 10' to 12' was thought to be important to allow groups of people to walk comfortably on the path (Figure 8). The width of the primary pedestrian path also makes it possible for the path to be plowed by a small truck or tractor. If this separated pedestrian system is to be successful in keeping pedestrians out of the roadway, it must be kept clear of snow throughout the winter.

- A "pedestrian priority crosswalk" has been planned near the Holiday Inn where the path crosses to the south to avoid head-in parking at the Skool House and the Alhorn. This raised crosswalk keeps the pedestrian path at the same level as it crosses the street. The traffic is carried over the walkway by gently sloping ramps. Not only does this type of crosswalk give the

pedestrian priority; it provides visual interest while allowing traffic. Design consideration should be given to maintenance and snow-plowing needs.

- Once the pedestrian path passes the west end of the hospital, it crosses back over to the north side of the street again and is reduced in width. Shared use of the roadway by buses and pedestrians is planned for the remainder of the corridor west to the Library/Ice Arena Plaza. The walkway from the hospital to the Library/Ice Arena Plaza would not be separated from the street by a curb.
- Seating is provided at regular intervals and at bus stops. Opportunities for public art should also be considered at these points. The seating must be designed with careful attention to location, views, landscaping, etc., to insure the seating will be used.
- A neighborhood entry feature has been designed at the beginning of West Meadow Drive, near the fire station, to act as a psychological deterrent to unnecessary vehicular traffic. The fire station apron provides an opportunity for these cars to turn around. While this treatment will only discourage the lost visitor from continuing west on Meadow Drive, it was felt to be a reasonable compromise between a card-activated gate or manned control gate.
- A pocket park is proposed on the Ski Museum site, creating a sense of pedestrian entry for the corridor and a better visual connection to East Meadow Drive. The park would include needed public restrooms, seating, and extensive landscaping.
- The Plan calls for extensive right-of-way landscaping to soften the corridor and reflect the landscape character of nearby Gore Creek. The landscaping should be used to add as much visual interest and natural character to the sidewalk and seating areas as possible. Dense plantings are proposed to create a sense of enclosure for this pedestrian corridor. Some landscaping on private property will be necessary to achieve this effect. Additionally, planters have been proposed east of the fire station to help define the roadway and reduce the amount of asphalt.
- A handicap bus stop has been added at the main entry to the hospital.

## COSTS

The cost estimate for this sub-area does not include complete resurfacing of West Meadow Drive's roadway. Improvements to buried utility systems and sub-surface drainage improvements are not included in this estimate.

Demo asphalt	26,600 sf x \$4.60/sf	\$119,700
Demo softsurface (includes repairing existing landscape after construction)	19,900 sf x \$1.25/sf	\$24,900
Patch asphalt	725 sf x \$2.20/sf	\$1,600
Curb and gutter	3,150 lf x \$131f	\$41,000
Concrete unit pavers	29,250 sf x \$6.00/sf	\$175,400
Widen library bridge	Lump Sum	\$20,000
Lighting	7 x \$2,500	\$17,500
Benches	9 x \$600	\$5,400
Trash receptacles	4 x \$450	\$1,800
Newspaper dispensers (6 per cluster)	2 x \$2,500	\$5,000
Deciduous trees (includes irrigation)	160 x \$300	\$48,000
Coniferous trees (includes irrigation)	100 x \$350	\$35,000
Shrub beds and irrigation	4490 sf x \$3.00/sf	\$13,500
Signage	Lump Sum	\$6,000
<b>Subtotal</b>		<b>\$519,800</b>
Relocate utilities	15%	\$77,070
		<b>\$596,870</b>
Miscellaneous contingency	15%	\$88,630
		<b>\$679,600</b>
Design & construction management	20%	\$135,900
<b>Total</b>		<b>\$815,400</b>

Note: It is essential that maintenance budgets be increased, especially snow plowing, to cover the cost of adequately maintaining the new streetscape improvements.

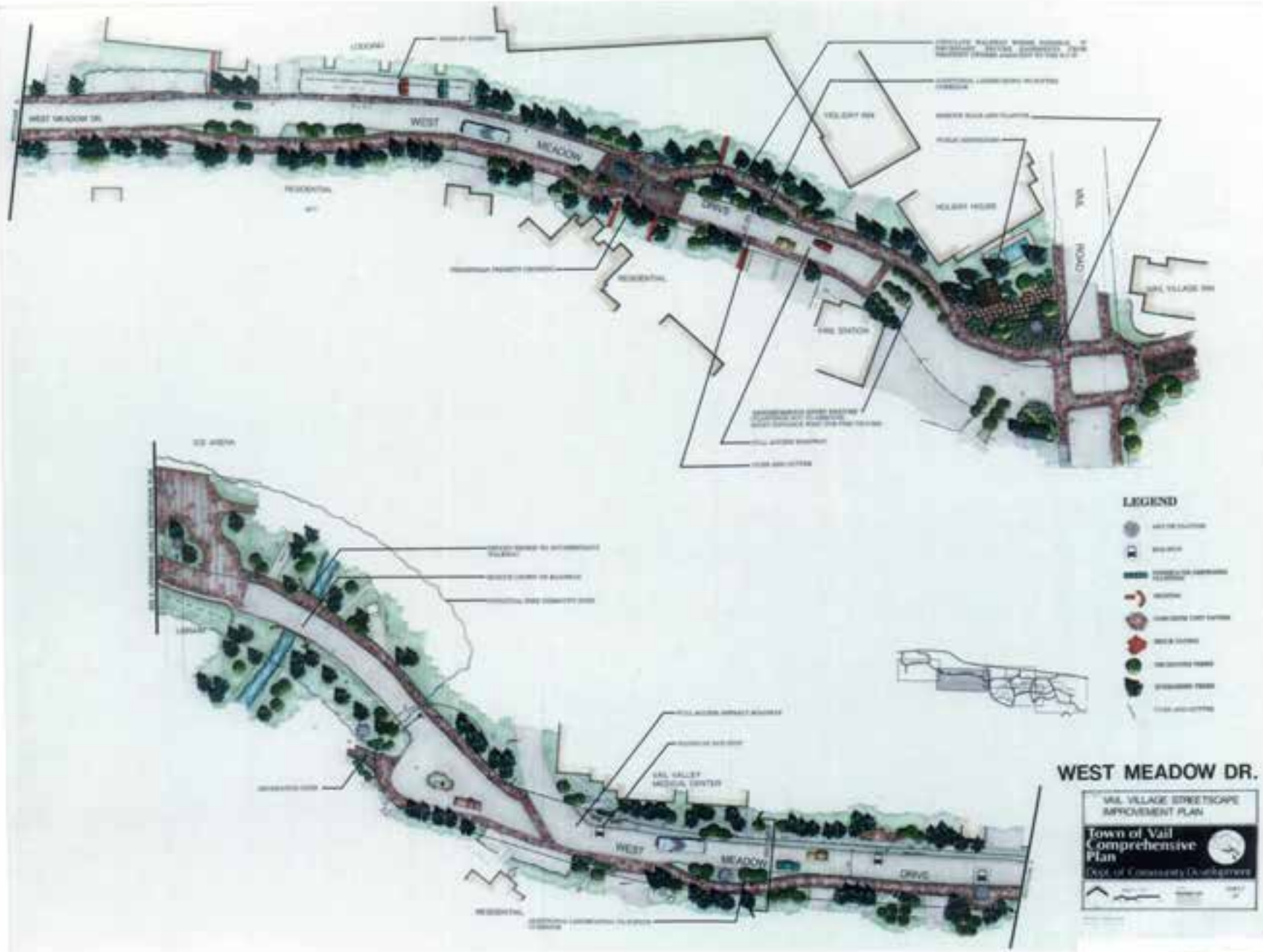


Figure 6

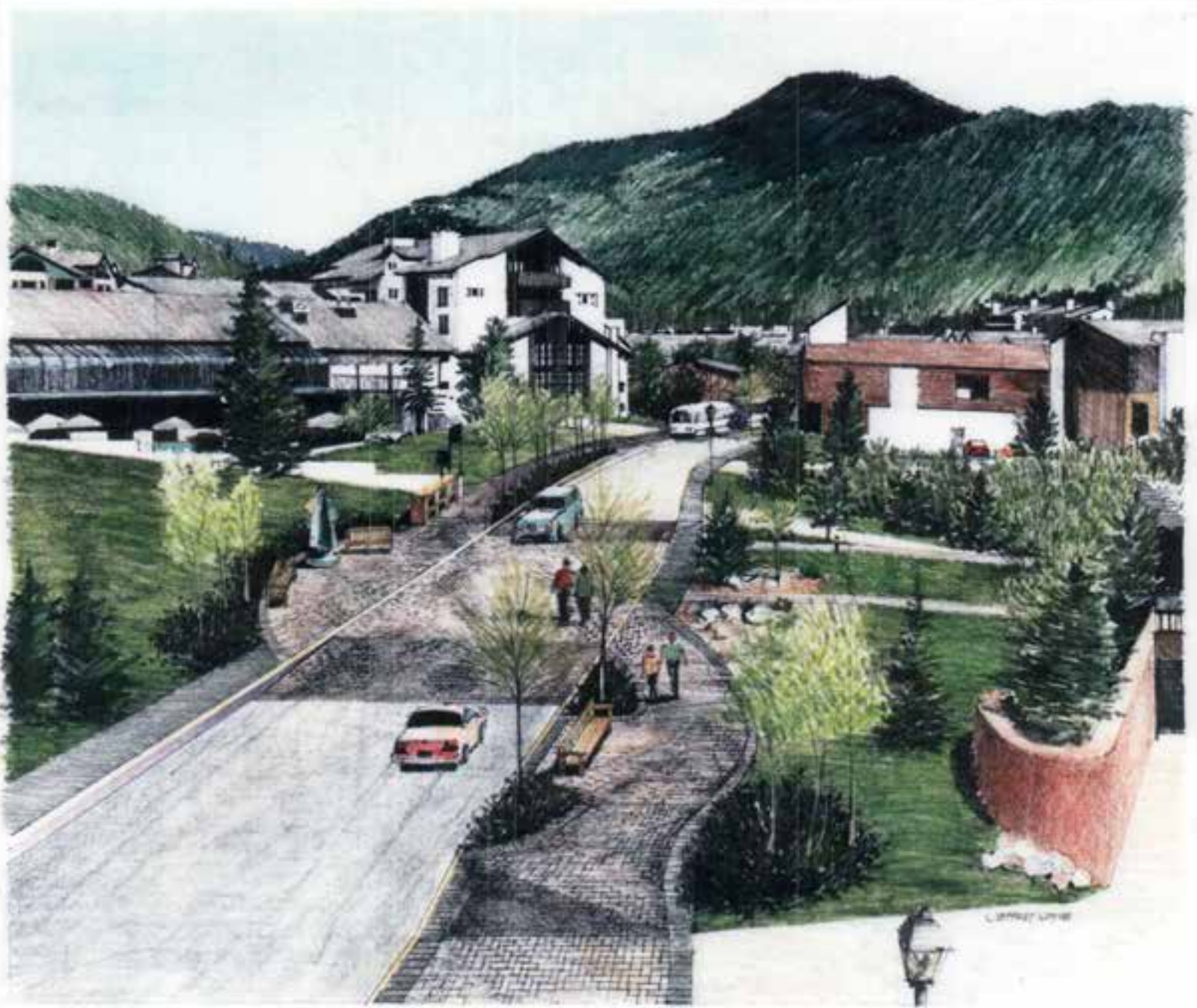
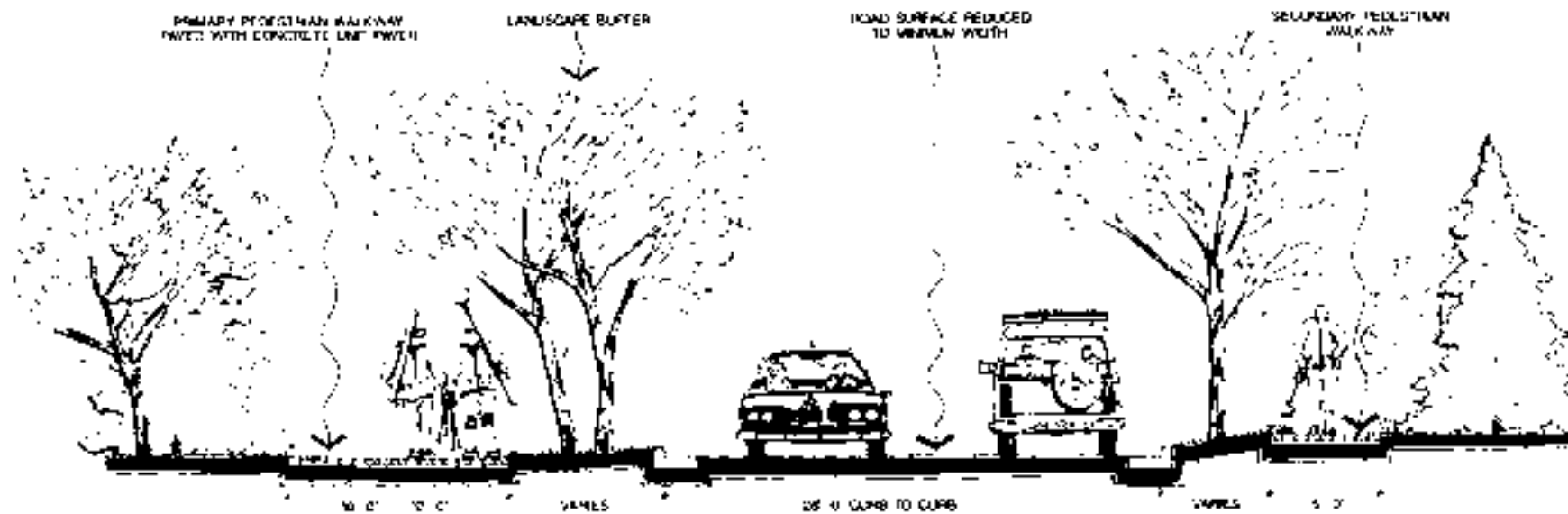


Figure 7



SECTION B  
WEST MEADOW DRIVE

Figure 8

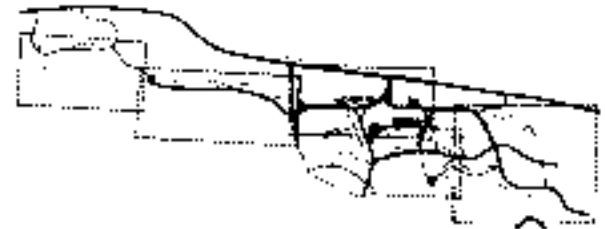
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## EAST MEADOW DRIVE - VAIL ROAD TO WILLOW BRIDGE ROAD

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### Existing Conditions

East Meadow Drive has been divided into two sub-areas as the east half and the west half have very different characters. The following section covers the western half of East Meadow Drive, from Vail Road to Willow Bridge Road and Vail Road from East Meadow Drive to the South Frontage Road.



### CHARACTER

Although the west half of East Meadow Drive is somewhat isolated from the Village Core, the restricted vehicular traffic and the closeness of buildings to the street give this area an ambiance similar to the Village Core. This character will be reinforced as building infills occur on the south side of the street. As in many areas in Vail, pedestrians enjoy walking in the street. The primary differences between this section of East Meadow Drive and the Village Core are the presence of bus traffic and the lack of enclosure on the south side of the street.

The streetscape character in this sub-area is further enhanced by the presence of two pedestrian plazas located at the Vail Village Inn (VVI) on the north side of the street. The combination of angled building alignments, stone walls, decorative paving, mature landscaping and gentle changes in grade make these plazas attractive focal points for pedestrians using East Meadow Drive.



Vail Road's character is changing from a street without pedestrian amenities and landscaping to one that has more pedestrian character. With the new Vail Gateway project and the Town's plans for landscaped medians and sidewalks along Vail Road, this transformation should be completed in the near future.

## CIRCULATION

### *Vehicular*

This portion of East Meadow Drive is restricted to Town buses by control gates. Even with the control gates, it is not unusual to see a lost visitor waiting in their car for the gate arm to be raised. There are two bus stops in this area (Figure 9) but the heavy pedestrian traffic reduces bus efficiency.

The existing buildings and plazas were constructed very close to or actually in the public right-of-way in this area, (Figure 9). Because of this alignment, and the presence of several large evergreen trees immediately adjacent to the roadway, there are areas where the public right-of-way may not be wide enough to accommodate the anticipated streetscape improvements. It would be best to formally change the right-of-way to correspond with the actual street alignment. If this is not possible, easements may be required for improvements on private property.

Vail Road carries large volumes of traffic and is anticipated to do so in the future. Because of the heavy traffic, pedestrians and vehicles should be separated.

### *Pedestrian*

On East Meadow Drive, pedestrians share the street with buses (and occasional service or emergency vehicles). Pedestrian volume is generally heavy in this area and often slows buses. Because this section of East Meadow Drive has a character that is similar to the Village Core, it is desirable to maintain this shared use of the roadway. Defining zones for the two uses is desirable.

Pedestrians now are forced to walk in the roadway along Vail Road, which is incompatible with the heavy vehicular traffic found there. The sidewalks presently being installed will alleviate this problem.

## PAVING/DRAINAGE

The existing paving, within the area bounded by the control gates, is primarily concrete with tinted concrete bands at regular intervals. The concrete is cracked and settling in spots and should be replaced. From the eastern control gate to Crossroads, the roadway is asphalt. Vail Road is also paved with asphalt.

The western VVI plaza is concrete with brick panels, and the eastern plaza is brick on sand. The paving treatments in these areas will not conflict with future streetscape improvements in the right-of-way.

There are storm drainage problems in the area that necessitate improvements to the underground existing storm drain system. As in East Lionshead Circle, the surface flows for this area will be from north to south. Some type of curb or barrier will be required along the south side of the street to intercept and direct these flows and its location and character will need to respond to the design proposed for this area by the Master Plan. As it is likely that this sub-area will be paved with a decorative paving material, a standard concrete curb and inlet system is inappropriate. Of the major utilities, only water service is proposed to be upgraded in the near future.

Vail Road will require an entirely new storm drain system that will also need to intercept flows from outside of the Streetscape Master Plan study area. Preliminary designs for Vail Road indicate that all streetscape improvements will be adjacent to the roadway and therefore, not tied to any subsurface improvement schedule.

## SITE AMENITIES

Private artwork is located in several of the plazas.

There are four wood-slat benches, one rustic wood bench at the bus stop and planter walls in the eastern-most plaza that are at seating height.

There are several Town-owned, wooden barrel trash receptacles located at the bus stops and at entry points to the VVI plazas.

No bike racks were noted in this sub-area.

## LANDSCAPING

A notable feature in this area is the lush landscape treatment of the berm along the Sonnenalp/Talisman parking lot, adjacent to the south side of the street. The berm is heavily landscaped and contains several large spruce, boulders, and flower beds. As future development occurs along the south side of the street, the major trees and screening should be maintained where possible.

There is also significant landscaping along the north side of the street, primarily in several large, Town-maintained planters. Stone planters in the VVI Plazas and around the outdoor dining decks are planted with annuals in the summer months.

This portion of East Meadow Drive has one of the richest landscape treatments in the Village.

With the recent improvements to the Alpine Standard and the Vail Gateway Building, the landscape character of Vail Road has improved. There is still a need for additional landscaping to soften the parking lots and drop-off areas along this street.

## UTILITIES

### *Lighting*

The predominate light fixture used in the area is a contemporary style, 12' tall, "cube" fixture with orange glass. This light fixture does not provide adequate light levels for the area and it would be positive to replace the "cube" lights with the "Village" light to achieve adequate lighting levels. This is mostly due to the orange glass that filters the light source. Lighting levels could also be improved by adding fixtures on the south side of the street.

Lighting on Vail Road is adequate at the intersection of the South Frontage Road. Moving south, the lighting levels decrease, especially on the west side of the street, and near the intersection with East Meadow Drive.

## PUBLIC COMMENT

Comments received included:

- It was believed that much of the Sonnenalp/Talisman's existing surface parking may remain, even with Sonnenalp redevelopment, therefore the major pedestrian corridor should be located along the north side of the street.

- The somewhat straight, south-side of the street should be given more variation with seating areas set off from the street.
- It may not be necessary to repave the entire area in "high quality" materials. Asphalt may be appropriate for some areas, like the bus lane.
- Most felt that the orange "cube light" fixtures along the Vail Village Inn project were inappropriate.
- The landscape berm along the south side of Meadow Drive should be preserved and enhanced.
- The proposed streetscape improvements should accent the existing pedestrian plazas.

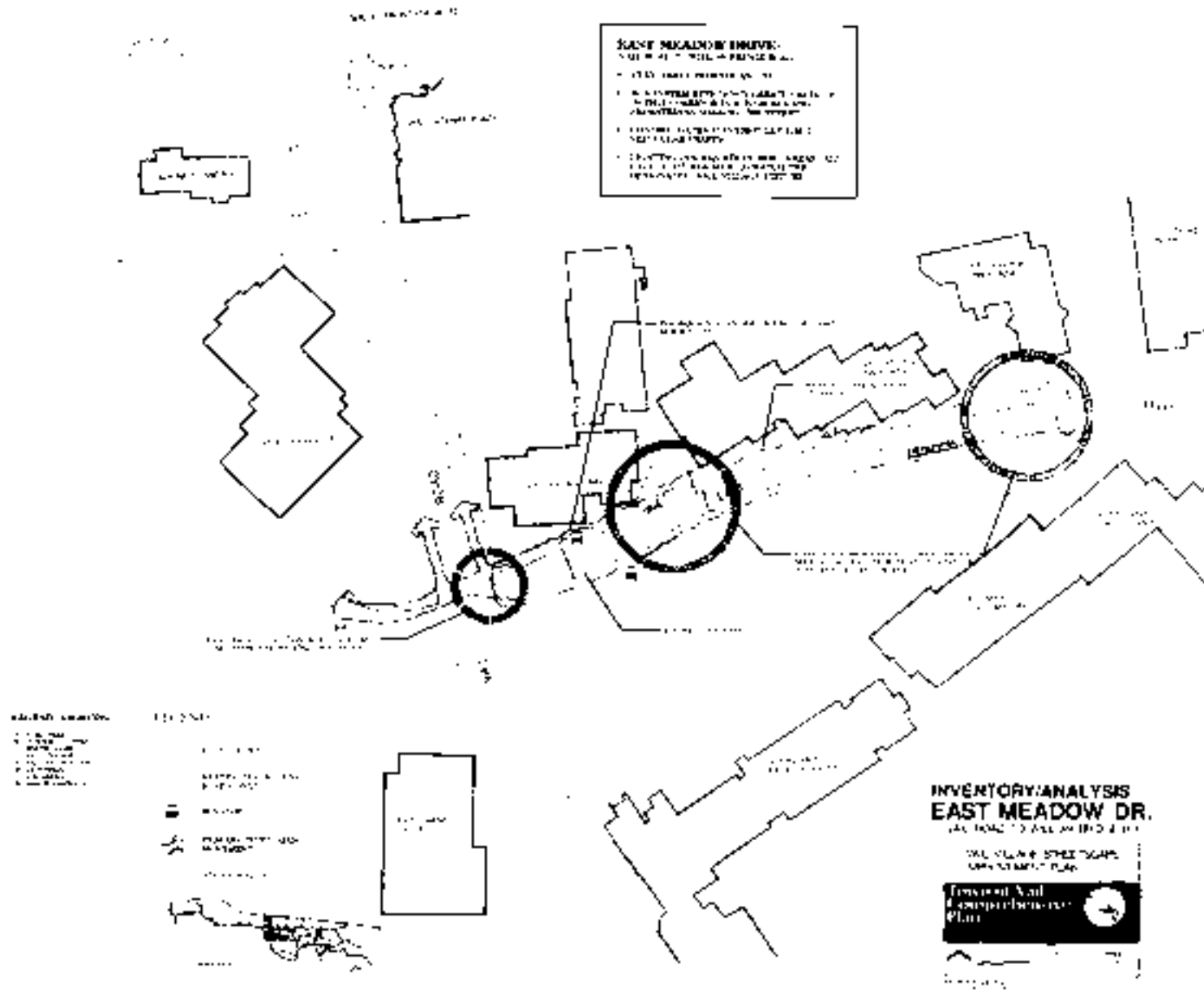


Figure 9

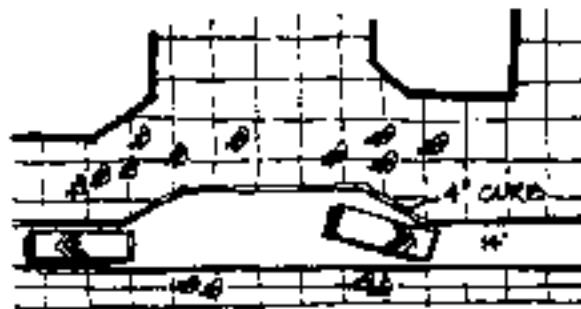
# Streetscape Improvement Plan

## *East Meadow Drive - Vail Road to Willow Bridge Road*

This west half of East Meadow Drive is significant because it is a heavily used pedestrian area that must also accommodate bus traffic. Accommodating and defining these two uses was the primary challenge presented by this sub-area. Other factors included enhancing the shopping experience on the north side of the street and planning for future development on the south side. All future developments proposed for this area should create interesting breaks in the linear character of the street and improve the pedestrian connection to West Meadow Drive and along Vail Road. Developments should not shade the street, nor should they "overwhelm" the street and the pedestrian areas with excessive mass and bulk.

### PRELIMINARY CONCEPTS

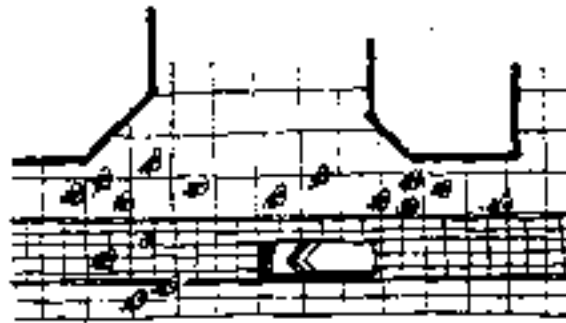
The conflicts between pedestrians and buses sharing the same roadway is not new to Vail. Until recently, however, the two uses have shared this section of East Meadow Drive without major conflicts. Serious problems are developing as the bus system reaches capacity and Vail's pedestrian usage increases. It was determined early on in the streetscape design process that the Master Plan needed to address this conflict as one of the sub-area's main goals.



CONCEPT A

The early concepts examined ways to define lanes for buses and pedestrians without detracting from the pedestrian-orientated character of the area. Three basic concepts were developed in response to this goal. One of the concepts has evolved into the final plan, and the two that were not explored further focused on the following approaches:

- *Concept A* Utilize low, 4' high, curbs to define the bus travel lane. Because the present roadway is not centered within the right-of-way, it was determined that there is not adequate room for two parallel bus lanes. Therefore, this concept used turn-outs at key points to allow buses to pass each other.



CONCEPT B

## PREFERRED STREETSCAPE PLAN

### *The Concept*

Concept A was rejected for two reasons: 1) It was felt that curbs, even low ones, degraded the pedestrian experience; and 2) Turn-outs would decrease the efficiency of the buses in this area.

- *Concept B* Keep the roadway at one elevation with no curbs to separate uses. Define the bus lane with asphalt paving and the pedestrian areas with concrete unit pavers. When east and west-bound buses needed to pass they would share the pedestrian area for a short distance. By providing lanes for each use, pedestrians would have a clear understanding of where they belonged as buses moved through the corridor.

While it was felt that the basic concept for this second scheme was workable, there was wide agreement that asphalt was not an appropriate material for this sub-area and that a more durable and aesthetically pleasing paving material was needed for the bus lane.

*Design Concept:* The preferred Streetscape Improvement Plan for East Meadow Drive -- Vail Road to Willow Bridge Road -- maintains the existing pedestrian-oriented character (Figure 10). It is felt that it is critical to maintain the opportunity for pedestrians to walk in the street. The proposed changes separate pedestrian and bus traffic by defining travel lanes with distinctive paving patterns and colors, rather than with a raised curb. Key areas are highlighted with special paving treatments and adjacent pedestrian plazas are incorporated into the Plan.

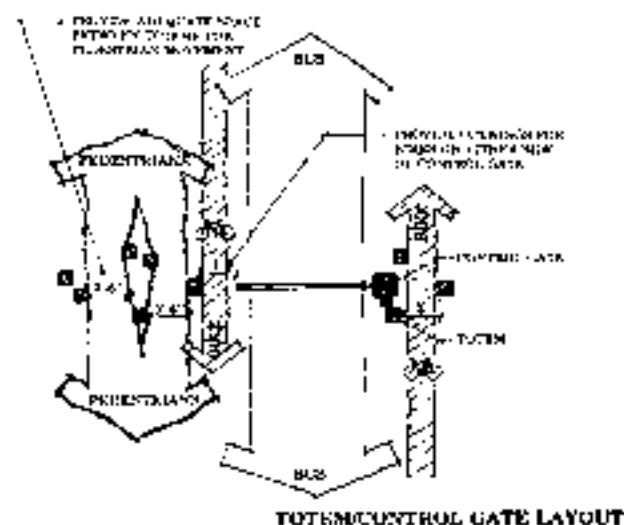
The basic circulation concept for this sub-area is to establish a 30' to 40' wide paved area for buses and pedestrians. Due to right-of-way constraints, some areas will be as narrow as 25' wide. The 14' wide bus lane would be on the south side of the paved area to allow for a major pedestrian zone on the north side of the street. A minor pedestrian zone would be provided on the south side of the street (Figure 11). The primary intent of this concept is to provide the pedestrian with a "safe" zone they can easily reach when bus traffic is moving through. This system has been used effectively in transit malls in other cities and increases both safety and bus efficiency, although in the winter, when the surface is snow covered, the problem will still exist.

Concrete unit pavers would be the primary paving material in the right-of-way. The bus lane will use a different style, color and pattern than pedestrian zones. The two zones will be separated by "soldier courses" of light colored unit paver bands or by a concrete edger. The end result will achieve pedestrian areas that flow from buildings on the north to those on the south side of the corridor, while also delineating the bus lane. Thus, the ability of pedestrians to "walk in the street" is maintained.

Six to eight-foot wide sidewalks are proposed along either side of Vail Road. Portions of the sidewalks have already been completed at the Vail Gateway Building and the Alpine Standard. All new walkways will be constructed with concrete unit pavers.

### Special Features

There are a number of features shown on the graphic for the Vail Road to Willow Bridge Road portion of West Meadow Drive. Most of these suggestions originated during the review of the Plan by the public and with Town boards and staff.



- The westbound Vail Village Inn bus stop has been moved further to the east to allow for more waiting room.
- The area around bus stops is highlighted by special paving treatments. The east and westbound bus stops are generally opposite each other, but there is room (20') for emergency vehicles to pass even with both buses parked at the bus stops.
- In response to comments that East Meadow Drive is too linear, the Plan proposes expanding the pedestrian zones north into the Vail Village Inn plazas and south to the Sonnenalp Swiss Chalet's main entry.
- Similar limitations in the selection of paving materials for adjacent private properties exist here as in the Village Core. The recommended materials and color ranges are discussed in detail in the Guidelines for Paving, Public Art, Site Furnishings, and Lighting section of this report.
- The control gate and totems at the intersection of Vail Road and East Meadow Drive should be reconfigured to better accommodate east/west



pedestrian and bicycle flow. If necessary, the bike lanes between the totems could be signed or striped as "bike lanes."

- The control gate on the east end of Meadow Drive should be moved to the east approximately 60-80 feet to prevent vehicular traffic from pulling into the east Vail Village Inn plaza.

### *Landscaping and Site Amenities*

The landscaping in this area is already very strong with a number of mature evergreen trees. The berm along the south side of the street is one of the richest landscapes in the Village and should remain so. Should the Sonnenalp redevelop, it will be important to maintain a strong landscape buffer, especially along parking areas to maintain the present character of the area.

There is a good balance between paving and landscaping on the north side of the street, hence no major landscaping is proposed for this area.

There are low walls, benches and outdoor dining areas along the north side of the street, therefore, only benches are recommended at the bus stops. The railroad tie retaining walls at the eastbound bus stop (south side of East Meadow Drive) should be replaced by boulder walls and landscaping should be added to improve the appearance of this stop. Seasonal benches, trash receptacles and bike racks could be located as conditions dictate. The eastern Vail Village Inn plaza has a need for more seating.

Landscaped medians are proposed for Vail Road. In addition to softening the corridor, it is hoped that these medians will make Vail Road appear to be less of a thoroughfare and discourage unnecessary vehicular traffic from turning on to it.

**COSTS**

The cost estimate shown below only includes costs for streetscape improvements in the right-of-way. It is anticipated that improvements on private property will be completed as properties redevelop or replace existing paving.

Demo asphalt	17,900 sf x \$4.50/sf	\$80,500
Demo softsurface	5,100 sf x \$1.25/sf	\$6,400
Patch asphalt	1,120 sf x \$2.20/sf	\$2,500
Curb and gutter	580 lf x \$13/lf	\$7,500
Concrete unit pavers	21,000 sf x \$6.00/sf	\$126,000
Lighting	10 x \$2,500	\$25,000
Benches	4 x \$600	\$2,400
Trash receptacles	4 x \$450	\$1,800
Newspaper dispensers (6 per cluster)	2 x \$2,500	\$5,000
Signage	Lump Sum	\$2,000
Deciduous Trees (inc. irrigation)	60 x \$300	\$18,000
Coniferous Trees (inc. irrigation)	20 x \$350	\$7,000
Shrub beds & irrigation	600 sf x \$3.00/sf	\$1,800
Move control gate	Lump Sum	\$10,000
Subtotal		<u>\$296,000</u>
Relocate utilities	10%	\$29,600
		<u>\$325,600</u>
Miscellaneous contingency	15%	\$48,840
		<u>\$374,440</u>
Design & construction management	20%	\$74,888
<b>Total</b>		<u><u>\$449,328</u></u>

Note: It is essential that maintenance budgets be increased, especially snow plowing, to cover the cost of adequately maintaining the new streetscape improvements.

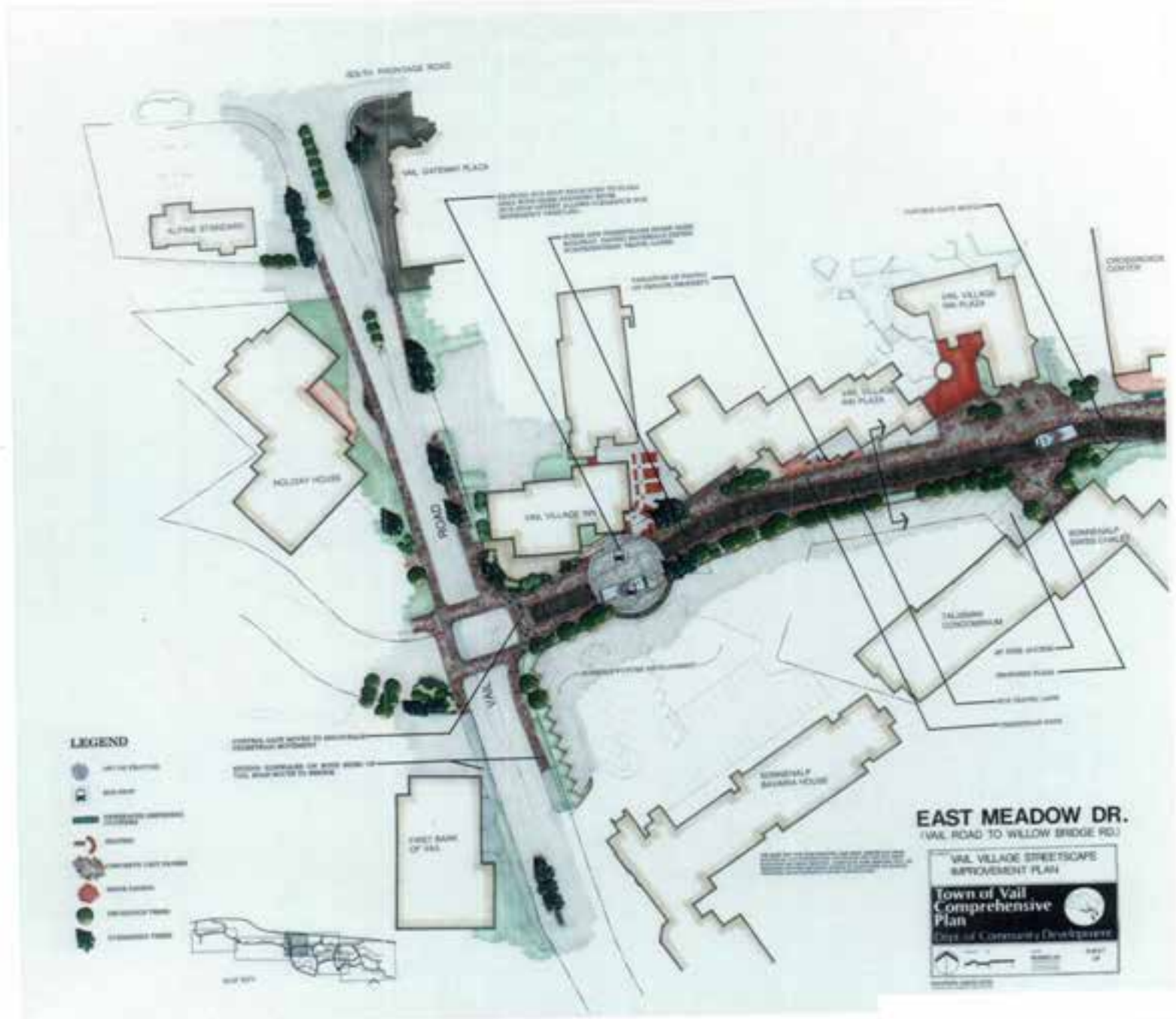
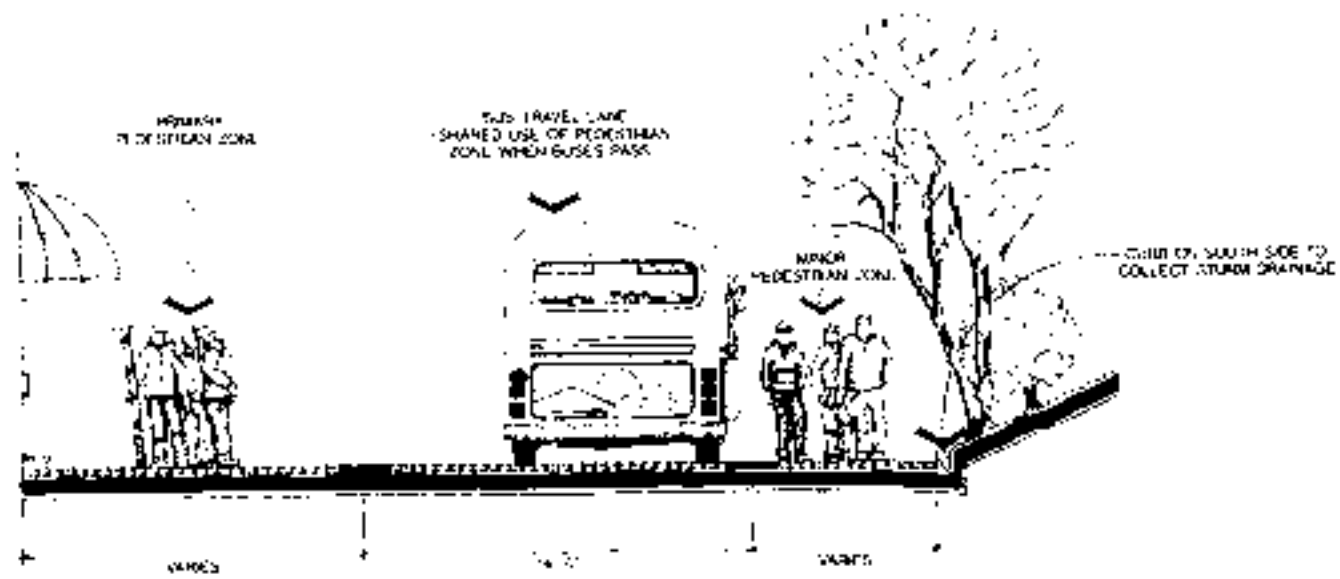


Figure 10



SECTION C  
 EAST MEADOW DRIVE  
 VAIL RD TO WILLOW BRIDGE RD

Figure 11

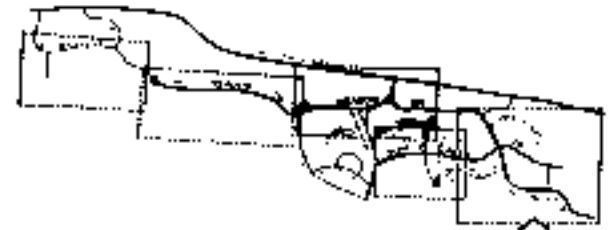
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## EAST MEADOW DRIVE - WILLOW BRIDGE ROAD TO VAIL VALLEY DRIVE

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### Existing Conditions

This sub-area includes the east half of East Meadow Drive from Willow Bridge Road to Slifer Square and extends east to Blue Cow Chute. It also includes Village Center Road, Willow Bridge Road south to the bridge and all of Slifer Square. The diversity of the uses and streetscape treatments suggest that the plan for this area address a wide-range of problems and issues. A graphic summary of the Inventory and Analysis of this sub-area is illustrated in Figure 12.



### CHARACTER

The character of this portion of East Meadow Drive is divided into two distinctive zones.

The east zone, from the intersection of Willow Bridge Road to Village Center Road is characterized by a standard street/curb/sidewalk section, the presence of vehicular traffic, varied building setbacks and a wide variety of landscape treatments.

East Meadow Drive, (Village Center Road to Slifer Square) is dominated by the Village parking structure to the north. The large grass slope south of the parking structure, the lack of sidewalks and the Sonnenalp's parking next to the roadway does not result in a positive pedestrian experience. The problem is lessened

somewhat because the road is restricted to Town buses only. The parking structure's grassed slope does soften this corridor, but because it is not currently heavily planted, it does not have the positive impact on the street that it could.

Village Center Road was historically a primary access point to Vail and it was, until recently, used as a secondary entry to the Village parking structure. The most significant feature of this short street is its 10% grade. With the changes to the circulation system brought about by the parking structure renovation, Village Center Road is now oversized and invites unnecessary vehicular access into the Village.

Willow Bridge Road is one of the primary pedestrian connections between the Village Core and East and West Meadow Drive. In addition, the international flag display along this street is a widely recognized symbol of Vail. With the existing bridge scheduled to be replaced by a wider, pedestrian-oriented bridge, the overall character and function of the area will be improved.

Slifer Square is a pedestrian plaza, with a large fountain, two bus shelters and is framed on the east and west sides by mature trees. This is the primary gateway for visitors entering Vail Village (via the Covered Bridge). However, the plaza is beginning to show its age in some areas -- especially paving -- and should be upgraded as a part of the Master Plan.

## CIRCULATION

### *Vehicular*

Village Center Road, East Meadow Drive (from Willow Bridge Road to the Sonnenalp parking lot access) and Willow Bridge Road do not have restrictions to vehicular traffic at this time. East Meadow Drive from Village Center Road to Slifer Square is restricted to Town buses only. It has been noted at the public meetings that there is a problem with cars using East Meadow Drive at the bottom of Village Center Road as a skier drop-off. The access for Crossroads retail center is off the west side of Village Center Road. Most of the remaining vehicular traffic on East Meadow Drive consists of visitors driving to the short term lodging units, lost visitors and service and delivery vehicles. Nonetheless, this is enough traffic to cause congestion on East Meadow Drive and to cause conflicts with the Town buses.

### *Pedestrian*

With the exception of the Village Center and Crossroads Mall areas, pedestrians share the roadway with the Town buses. Because of the heavy pedestrian traffic moving to and from the Village parking structure, and the lack of a defined pedestrian zone, there are often conflicts.

Slifer Square sees very heavy pedestrian usage by people moving between the Village parking structure and the Village Core or the ski mountain.

### **PAVING/DRAINAGE**

The existing roadways in the sub-area are all asphalt. Slifer Square's paving is tinted concrete and the north half of the plaza is newer than the south half. The paving in the south half of the plaza has deteriorated to the point where it will need to be replaced soon. Unless the entire plaza is repaired at the same time, the tinted concrete in the north half will continue to not match the pavement in the south half.

There is a 6' wide concrete sidewalk around the southeast end of the Crossroads Mall area. The Village Center retail shops have a brick-paved pedestrian plaza that is separated from the street by landscaping. There is a street level brick walkway on the east side of Willow Bridge Road.

The Muller Engineering Drainage Report has indicated a need for storm drainage improvements for this sub-area. This will take the form of upgrading the existing system, primarily at the Sonnenalp. Where the proposed streetscape improvements are outside of the roadway, the drainage and streetscape work can be completed independently of each other. Where the streetscape improvements occur in the roadway, coordination will be necessary.

### **SITE AMENITIES**

The raised, brick walkway on the south side of the street at the Village Center features four wood-slat benches. There are also four "wooden barrel" trash receptacles here.

There are two, rustic wood-slab benches at the Crossroads Mall bus stop, but they are not close enough to the bus stop to be well-used. There are bike racks, trash receptacles and benches at the east end of Crossroads.

Slifer Square offers a wide range of site amenities that would typically be found in such an important pedestrian space. The two bus stops in the north end of the plaza have benches, trash receptacles and newspaper dispensers, but these do not match the site furnishings found elsewhere in the plaza. The bus stop on the north side of Slifer Square contains the "Children's Mural" - a colorful tile artwork depicting the history of Vail as seen through the eyes of Vail's children. When addressing the site amenities for Slifer Square, the Streetscape Plan should develop a comprehensive treatment for all elements.

## LANDSCAPING

The landscape treatments at the intersection of East Meadow Drive and Willow Bridge Road include railroad-tie planters adjacent to Crossroads and the Sonnenalp - Swiss House as well as a triangular stone planter in the center of the intersection. Railroad-tie planters lack durability and do not fit Vail's desired design character and should be replaced. The triangular, stone planter located in the center of the intersection is attractive, however, it is too small for the space and should be expanded.

The landscape treatments around the east end of Crossroads Mall and the Village Center are attractive and appropriate. No change will be needed in either location.

The Village parking structure's south facing hillside will have more variety and interest when the Town completes its planting plan. Additional landscaping is needed along the north side of the Sonnenalp - Austria House's parking lot for screening and to soften the corridor.

Slifer Square is surrounded by mature trees and shrubs. The only additional landscaping that is needed here will be done in conjunction with improvements to the Village parking structure.

## UTILITIES

Water service and storm sewer lines will need to be replaced in the near future in this sub-area. This work should be completed prior to beginning any streetscape improvements. No other major utility upgrades are anticipated.

### *Lighting*

A variety of lighting levels and fixtures types can be found in this sub-area. With the remodeling of the Village parking structure, its entries are now very well lit



by "Village" light fixtures. The "Village" fixture is also well distributed throughout Slifer Square, with only one or two additional fixtures needed. Additional lighting is needed at the intersection of Willow Bridge Road and East Meadow Drive, along the east end of Crossroads Mall and on East Meadow Drive between the west pedestrian portal of the Village parking structure and Slifer Square.

## **PUBLIC COMMENT**

Comments received regarding this sub-area covered a variety of issues. Most suggested that the separation of pedestrians and vehicles was generally working well in the Village Center/Crossroads area and that mixing Town buses and pedestrians could continue to work well by the Village parking structure, with a clearer definition of the travel lanes. Specific comments included:

- The Streetscape Plan should respond to the heavy pedestrian traffic that is found on East Meadow Drive along the Village parking structure. Some definition of pedestrian and bus lanes was needed.
- There is a serious problem with "drop-off" traffic at the bottom of Village Center Road.
- The Plan will need to maintain vehicular access to lodging units in the area.
- Additional seating opportunities are needed along the corridor.
- More landscaping is needed to soften the area, especially by the Sonnenalp's parking lot across from the Village parking structure.
- The short-term parking spaces that are located just north of Willow Bridge are often not available for service and delivery vehicles due to over use by "locals".

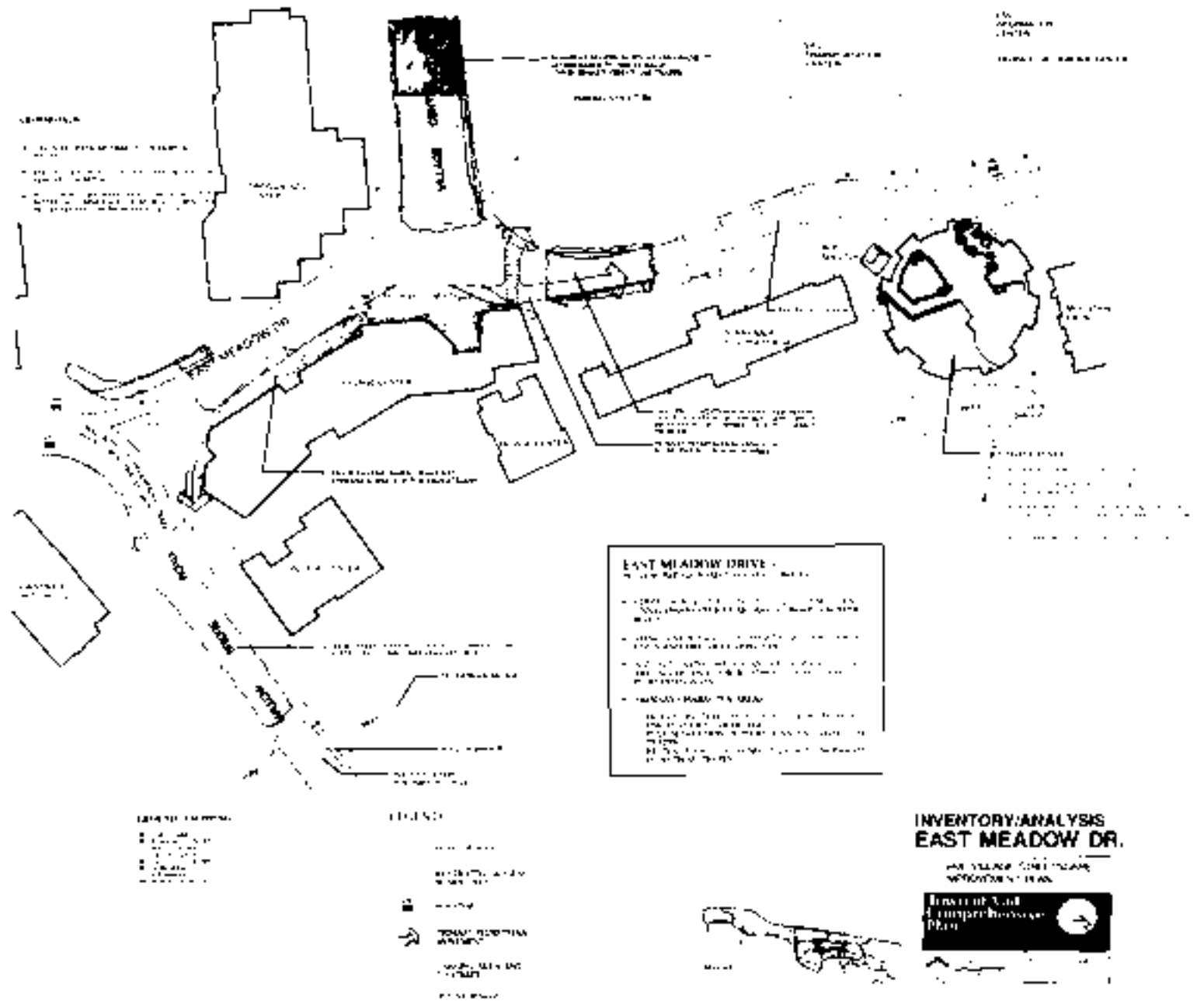


Figure 12

# STREETSCAPE IMPROVEMENT PLAN

## East Meadow Drive - Willow Bridge to Vail Valley Drive

The sub-area that encompasses the east half of East Meadow Drive also includes Village Center Road, Willow Bridge Road and Slifer Square. The improvements proposed for this sub-area address many of the same problems faced in all sub-areas of the Master Plan: separating buses from pedestrians, providing a comfortable shopping experience, controlling unnecessary vehicular traffic, maintaining vehicular access to lodge units in the area, screening parking and accommodating service and delivery vehicles. The Streetscape Plan for this sub-area (Figure 13) is unique as it utilizes most streetscape treatments that are proposed in other sub-areas of this Master Plan.

### PRELIMINARY CONCEPTS



Unquestionably, the major area of discussion during the development of the Master Plan for this sub-area was the issue of whether to add a manned information booth/control gate somewhere on Village Center Road. The purpose of this feature would be to help direct visitors to their destination without entering the pedestrianized areas of the Village and to serve as another source of information for the visitor to Vail.

During the development of the plan, the information booth was sited in a number of locations:

1. Just north of the entry to Crossroads's parking garage. This location was rejected due to the steep grade of the street, a lack of stacking area for cars entering Crossroads and operational problems of handling both Village traffic and cars bound for Crossroads.
2. At the intersection of Willow Bridge Road and East Meadow Drive. This location was eliminated because it allowed vehicles into the bus route and would not reduce skier drop-off traffic at the bottom of Village Center Drive.
3. At the south end (the bottom) of Village Center Road. This location worked well for stopping unnecessary traffic before entering the bus lanes and for

avoiding conflicts with Crossroads, but the steep, downward slope of the road made this location undesirable.

4. The last proposed location was up on the South Frontage Road, in the eastbound deceleration lane for Village Center Road. This location achieved most of the needed objectives, but was discarded as it was felt that it duplicated the information booth at the nearby Transit Center because of the lack of right-of-way.

While there was agreement that the concept of a control gate/information booth in this general location was a good one, the physical limitations and the potential for operational problems removed the concept from further consideration.

In the end, it was felt that narrowing the road surface of Village Center Road, by increasing landscaping on each side of the road, and adding signage stating that the Village was a pedestrian area, would de-emphasize this street and create a psychological deterrent to unnecessary traffic.

One other idea suggested for East Meadow Drive but not included in the final plan, was to close Village Center Road south of the Crossroad's entry. This idea was rejected because it would force all traffic, with a legitimate purpose, (Village Center/Sonnacnalp guests and service and delivery vehicles) down Vail Road to Check Point Charlie and over the Willow Bridge, adding to the traffic problems in that area.

## **PREFERRED STREETSCAPE PLAN**

*Design Concept:* The final plan for East Meadow Drive, from Willow Bridge Road to Slifer Square, proposes some significant changes to the character of the streets in this sub-area. (Figure 13) Where the majority of the streets are currently auto-orientated, only one short section of East Meadow Drive would retain that character. For all other portions of the streetscape, the improvements have targeted enhancing the pedestrian environment and reducing the emphasis on the car.

The Plan also attempts to improve the function and character of the area by adding sidewalks where none presently exist, screening parking areas and replacing railroad-tie retaining walls with boulder or stone walls.

### *Vehicular*

East Meadow Drive, Village Center Road and Willow Bridge Road would remain open to traffic. East Meadow Drive along the Village parking structure would still be restricted to buses only. There are three revisions to the vehicular circulation in this sub-area:

- The existing control gate and totems on East Meadow Drive, near the west portal of the parking structure, will be relocated further to the west near the Sonnenalp's parking access. This eliminates the existing dead-end situation.
- The one-way traffic designation that now begins at the north end of the Willow Bridge will be moved further to the north and just south of the Village Center parking access. This will decrease the number of vehicles turning around in the road and will minimize traffic conflicts with pedestrians.
- The 15-minute parking spaces north of the Willow Bridge on the west side of the street will be designated as service and delivery spaces, per the recommendations of the Transportation Master Plan. These spaces will be distinguished from the pedestrian zones with a dark colored concrete unit paver.

### *Pedestrian*

Pedestrian circulation is proposed to be handled in a number of different ways:

- For Willow Bridge Road, the pedestrians would share the 20' to 25' roadway with the vehicular traffic using that road.
- For East Meadow Drive:
  - The area west of the intersection of Willow Bridge Road would be treated the same as the west half of East Meadow Drive, with the 14'-wide bus lane and street-level pedestrian zone (see previous section).
  - From Willow Bridge Road to the control gate at the west end of the parking structure, the pedestrians would be separated from vehicular traffic. An 8' - 10' wide walkway is called for on the Crossroad's side of the street while the existing raised brick walkway in front of the Village Center will remain as is. The street would remain paved in asphalt.

- From the control gate east to Slifer Square there would be separate lanes for pedestrians and buses, but without a grade separation. The north side of the street will be dedicated to a 14' wide bus lane and the south side would include a 12' to 15' wide pedestrian way. Should the Town decide to utilize the existing right-of-way in the area of the Sonnenalp's Austria Haus, an additional pedestrian walkway is proposed on the north side of the road. Additional landscaping should also be added on the south side of the street to further screen the Austria Haus parking lot.
- A new walkway has been constructed on the north side of the street from Slifer Square east to Vail Valley Drive and the South Frontage Road. The control gate just east of Slifer Square should also be relocated to the intersection of Vail Valley Drive. This restriction of traffic will have to be coordinated with the Mountain Haus and the Vail Athletic Club.

*Special Features*

The key elements of the Streetscape Improvement Plan for this sub-area include:

- The asphalt road surface would be replaced by concrete unit pavers in most areas where pedestrians share the street with vehicles. In the restricted access zone south of the Village parking structure, the pedestrian path would be paved with concrete unit pavers. However, the bus lane would remain asphalt. Additionally, the Willow Bridge Road street surface, from the north side of Willow Bridge to the intersection of East Meadow Drive, would also remain asphalt.
- Replacing the triangular planter at the intersection of East Meadow Drive and Willow Bridge Road with a larger circular planter that is centered in the intersection. In addition, the intersection will be accented with a special paving treatment and will act as a focal point.
- Village Center Road would be narrowed to a minimum width of 28' (curb-to-curb) to discourage unnecessary traffic. Five to six foot wide concrete sidewalks on each side of the street are planned to connect East Meadow Drive to the Frontage Road.

- Unit paver crosswalks would connect the new pedestrian system along East Meadow Drive to the west portal of the Village parking structure.
- Allowances would be made for the continuation of the streamwalk path. This would involve a pedestrian-only walkway under the new Willow Bridge and either a ramped walkway or steps up to Willow Bridge Road.
- The design of Slifer Square is essentially unchanged, except for repaving the entire plaza with one of the specialty paving materials as the existing concrete surface wears out. At the time of repaving, special consideration should be given to examining opportunities for creating a special events location. The site has the potential to be slightly adjusted to better handle the Christmas Tree Lighting Ceremony and other public events and ceremonies without losing its attractiveness. Consideration should be given to the removal of some of the existing stone walls around the fountain to restore its original design and to allow access to the water. It is important that a comprehensive design analysis of Slifer Square occur before any changes would be made, as it is one of Vail's most loved public settings. Seating and lighting should also be re-examined. Opening up the Square to the creek and accommodating the existing (east-bound) and proposed (west-bound) streamwalk paths are suggested.

### *Landscaping and Site Amenities*

Approximately one-half of this sub-area is already well-landscaped. The existing, raised brick walkway that is adjacent to the Village Center commercial area is a good example of an area that is well-landscaped and well appointed with site furnishings. Some of the improvements suggested in the Plan include:

- A strong landscape screen along East Meadow Drive, between the pedestrian path and the Sonnenalp-Austria Haus' parking lot.
- The railroad-tie planters along the parking lots for Crossroads and the Sonnenalp-Swiss House should be replaced with at-grade planting beds, similar to the planting beds along the south side of East Meadow Drive near the Vail Village Inn.

- The berm that screens the south side of the Village parking structure should be heavily planted. Snow storage requirements for the structure should be accommodated.
- Landscaping is needed along Village Center Road, especially near the Frontage Road, so as to further inhibit vehicular traffic into the pedestrianized areas.
- Landscaping, benches and a trash receptacle should be placed at the Crossroad's bus stop as shown on the Master Plan graphic.
- Expanding the planter on the south side of the intersection of Village Center Road and East Meadow Drive has been suggested. This would provide better definition to the roadway as well as increasing the landscaped area.
- Pocket parks are planned on either side of the new Willow Bridge. These could be built in conjunction with the implementation of the Streamwalk, with the reconstruction of Willow Bridge or with the redevelopment of adjacent properties.



## COSTS

This cost estimate does not include landscaping for the Village parking structure. Improvements to underground utilities and private property are also excluded from this estimate.

Demo asphalt	51,950 sf x \$4.50/sf	\$233,700
Demo softsurface	4,920 sf x \$1.25/sf	\$6,200
Curb and gutter	660 lf x \$13.00/lf	\$8,600
Concrete unit pavers	32,200 sf x \$6.00/sf	\$193,200
Asphalt Patch	1,400 sf x \$2.20/sf	\$3,100
Lighting	7 x \$2,500	\$17,500
Benches	5 x \$600	\$3,000
Trash receptacles	5 x \$450	\$2,200
Newspaper dispensers (6 per cluster)	1 x \$2,500	\$2,500
Deciduous trees (includes irrigation)	72 x \$300	\$21,600
Coniferous trees (includes irrigation)	92 x \$350	\$32,200
Shrub beds & irrigation	4,420 sf x \$3.00/sf	\$13,300
Signage	Lump Sum	\$2,000
Control gate moved	Lump Sum	\$10,000
Subtotal		<u>\$549,700</u>
Relocate utilities	10%	\$55,000
		<u>\$604,700</u>
Miscellaneous contingency	15%	\$90,700
		<u>\$695,400</u>
Design & construction management	20%	\$139,100
<b>Total</b>		<u><u>\$834,500</u></u>

Note: It is essential that maintenance budgets be increased, especially snow plowing, to cover the cost of adequately maintaining the new streetscape improvements.



Figure 13

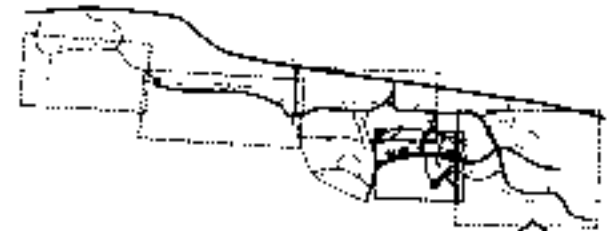
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## THE VILLAGE CORE

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### Existing Conditions

The narrow pedestrian streets, plazas incorporating landscape features and fountains, the distinctive "Tyrolean-style" architecture, the outdoor dining decks and the attractive side streets such as Wall Street and the Gore Creek Promenade all combine to create the Village's distinctive character. This sub-area includes Gore Creek Drive, Bridge Street, the Gore Creek Promenade, the south half of

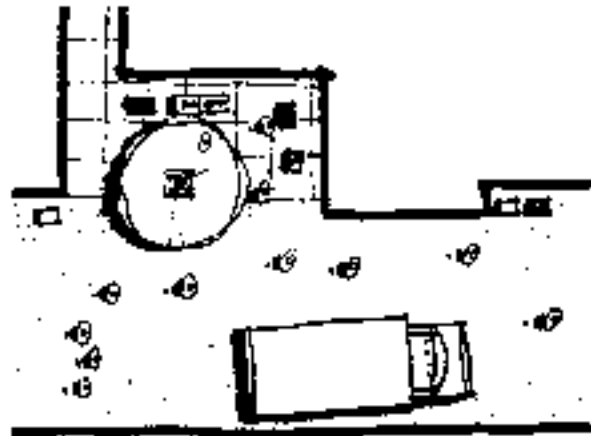


Willow Bridge Road and the Mill Creek Court area. A graphic summary of the Inventory and Analysis of the Village Core is shown in Figure 14.

### CHARACTER

While the architecture and urban framework of the Village Core creates a very unique setting, the existing streetscape treatment does not quite measure up to the same standards:

- Asphalt is the primary paving material in the public areas of the Village.
- Seating is often hard to find and, where there are benches, there is a wide range of styles used.
- Street lighting is not consistent. Some areas are very well lit, while many others are not. A variety of lighting fixtures can be found.



- There is a need for additional landscaping to help soften portions of the Village which are predominately asphalt streets, stone walls and building facades.
- Site furnishings, such as newspaper dispensers, are now scattered throughout the Village, often times located in the center of our most prominent plazas. These should be reduced in number and clustered. Many areas are lacking bike racks and trash receptacles.
- Service and delivery vehicles too often add clutter and congestion to the street setting.

However, even with these shortcomings, Vail Village is one of the premier pedestrian spaces in the country. The Plan's intent is to strengthen aspects of the Village that already create a wonderful diverse streetscape experience.

## CIRCULATION

### *Vehicular*

The Village Core is commonly known as a "pedestrian village". This is, in many ways, a misnomer. While vehicular access is restricted by Checkpoint Charlie, it is highly unusual to walk through the Village without encountering a car or a service/delivery vehicle. It is not possible, or perhaps even desirable, to totally eliminate vehicles in the Village. It is, however, desirable to preserve the pedestrian environment and to reduce the number of vehicles that are presently entering the Village Core.

Concurrent with the Streetscape Master Plan, the Town is also preparing a Master Transportation Plan. If the recommendations of the Master Transportation Plan are adopted and enforced, the goal of reduced traffic in the Village should be accomplished. The recommendations of the Master Transportation Plan that affect the Streetscape study area have been incorporated into this document.

The public right-of-way should not be a limiting factor in the Streetscape Master Plan for the Village Core. However, it is likely that some public improvements will need to be made on private land, where easements may be needed to accomplish

## *Pedestrian*

the objectives of the Master Plan. Adequate access and set-up areas for fire and emergency vehicles need to be maintained.

Pedestrian usage throughout the Village is heavy and generally unrestricted. This unrestricted use of the Village streets will be maintained and encouraged by the recommendations of the Streetscape Master Plan. There are a number of ways in which the pedestrian experience can be improved:

- Gore Creek Drive, in front of The Lodge at Vail, is made unnecessarily "hard" by the presence of a stone wall between the raised sidewalk and the street. The wall limits pedestrian access to the retail shops along the street, but does help separate the service and delivery vehicles from the sidewalk. With the removal of these service and delivery spaces, as recommended in the Master Transportation Plan, the need for this wall is eliminated.
- The Mill Creek Court building is not well connected to the Village Core. The Streetscape Master Plan includes this area and promotes the creation of other connections across Mill Creek from Bridge Street (possibly between the Rucksack Building and the Red Lion Building.)
- Restore the Children's Fountain to its original design, by allowing access to the water. The original design did not include the existing stone wall now surrounding the fountain.

## **PAVING/DRAINAGE**

In the public right-of-way, asphalt is the primary paving material. Currently, most of the pavement surface is in good condition. However, asphalt as a paving material is not appropriate for such a major pedestrian space. There are other richer, more durable materials which will be recommended as a part of the Master Plan.

One of the major issues the Streetscape Plan must address is how paving on private property is to be handled. There is currently a wide range of paving materials used next to the public right-of-way. This plan will provide a means to integrate private improvements, with improvements in the public right-of-way, so that the design goal of a cohesive pedestrian environment is achieved.

Another key goal of the Master Plan will be to develop a pavement treatment that accentuates Vail's unique architecture and key public spaces in the Village Core.

The Village Core is almost entirely made up of impervious materials that have resulted in a number of storm water run-off problems. All of the precipitation that falls in this area must be accommodated by an underground storm sewer system. The main problems occur on Bridge Street south of Gore Creek Drive. Wall Street will also require storm sewer improvements to intercept storm flows that come off Vail Mountain. In addition, there are a number of low spots along Gore Creek Drive where storm water collects, such as in front of the Lazier Arcade Building.

Because the Streetscape Plan will include improvements in the right-of-way, any storm sewer work should be completed prior to upgrading paving. In addition, if curbs are needed in the Village Core, they should be incorporated into steps or planter walls and constructed from materials other than concrete or asphalt.

## **SITE AMENITIES**

The existing site furnishings and amenities in the Village Core represent a mix of styles and materials. To date, most public seating has been provided by the individual property owners. During the summer months, the Town places benches around the Children's Fountain, a prominent public artwork, and near the Casino Building. Overall, the distribution of seating is not well spaced or well planned. Other site furnishings are also missing. For instance, bike racks are almost nonexistent in the Village Core.

The Streetscape Plan will propose additional seating opportunities, clustered newspaper dispenser locations, bike racks and trash receptacles.

Just to the south of the Village Core along Mill Creek, Pirate Ship Park offers a unique amenity for children. Better signage making people aware of the facility is needed.

## **LANDSCAPING**

Most of the landscaping in the Village Core has been provided by the private sector, as individual properties in the Village have redeveloped. Because there are no building setbacks in the Village, many properties have constructed buildings

right up to their property lines. Hence, many of the private landscaping improvements are located on public property. Additionally, there have been public landscape improvements in the Gore Creek Promenade, Earl Eaton Plaza and Seibert Circle.

The existing Village landscape treatment consists mostly of small planters with rock walls. This plan will propose additional pockets of landscaping, where possible. Less obvious solutions, such as hanging baskets of flowers on buildings or light poles will also be needed. Unfortunately, because of restrictions imposed by fire access requirements and the locations of the existing buildings, it will not be possible to make major additions to the landscape character in this area.

## UTILITIES

Some below ground utilities will need to be updated soon, such as water, storm drainage and cable TV. The appropriate agency or public utility has been consulted as a part of this Master Planning process and their plans will be incorporated into the sequencing of the Master Plan recommendations. Another key area of concern regarding storm drainage will be to develop an aesthetically pleasing replacement for concrete curb and gutters as a means of conveying storm water into the inlets.

### *Lighting*

Street lighting in the Village has been implemented in a piecemeal fashion, as properties have redeveloped and as the Town has phased in additional lights. The result is that some areas, such as upper Bridge Street and the Wildflower Restaurant access, are very well-lit while the west end of Gore Creek Drive is dark and uninviting. This Master Plan will address lighting on a conceptual level and propose solutions. It will still be necessary to complete detailed lighting plans for the Village Core, as well as all the areas covered by this Master Plan, before adding lighting.

## PUBLIC COMMENT

There was significant comment from the public regarding the Village Core. Most of the comments and ideas listed below were focused directly on the Village Core, while others could be applied to other sub-areas included in the Master Plan. The key comments addressed were:

- A desire to restore the Children's Fountain to its original design concept which removes the walls and allows easy access to the water.
- General agreement that lighting is inadequate in the Village Core. Also, that the Town should consider accent lighting key features such as the Covered Bridge.
- There was agreement that having more places for artisans and performers to play in the Village was an excellent idea.
- It was noted that the Town's unique history was generally ignored. All of the key plazas and landmarks in the Village should have crafted signs or plaques that give the location's name and history. This could even extend to privately-owned buildings. Some felt that paving accents could serve to guide the visitor to these points of interest in the Village and other areas of the Town. View corridor origin points could be marked by a decorative marker placed in the pavement.
- All agreed that conflicts between bikes, rollerblades and other recreational uses were increasing.
- Most preferred the idea of high quality paving materials for the Core, but there was concern that the proposed treatment strike a balance between being too busy and being sterile. Most felt that the public right-of-way line should not be well defined in order to maintain the feel of the pedestrian being able to walk in the street.
- All agreed that the small pedestrian bridge over Gore Creek, located just east of Willow Bridge, should be retained during the proposed 1992 reconstruction of Willow Bridge.
- More landscaping was recommended to soften the streetscape environment. All liked the hanging flower baskets. It was suggested that more trees be planted along Mill Creek.



- The grassed park area near the Gore Creek Promenade needs to be regraded to drain properly, to allow people an opportunity to sit on the grass -- low flat rocks could be used for seating as well.
- Public restrooms are needed in the Core, as well as other areas of the Village.
- All proposed designs should allow for handicap access.

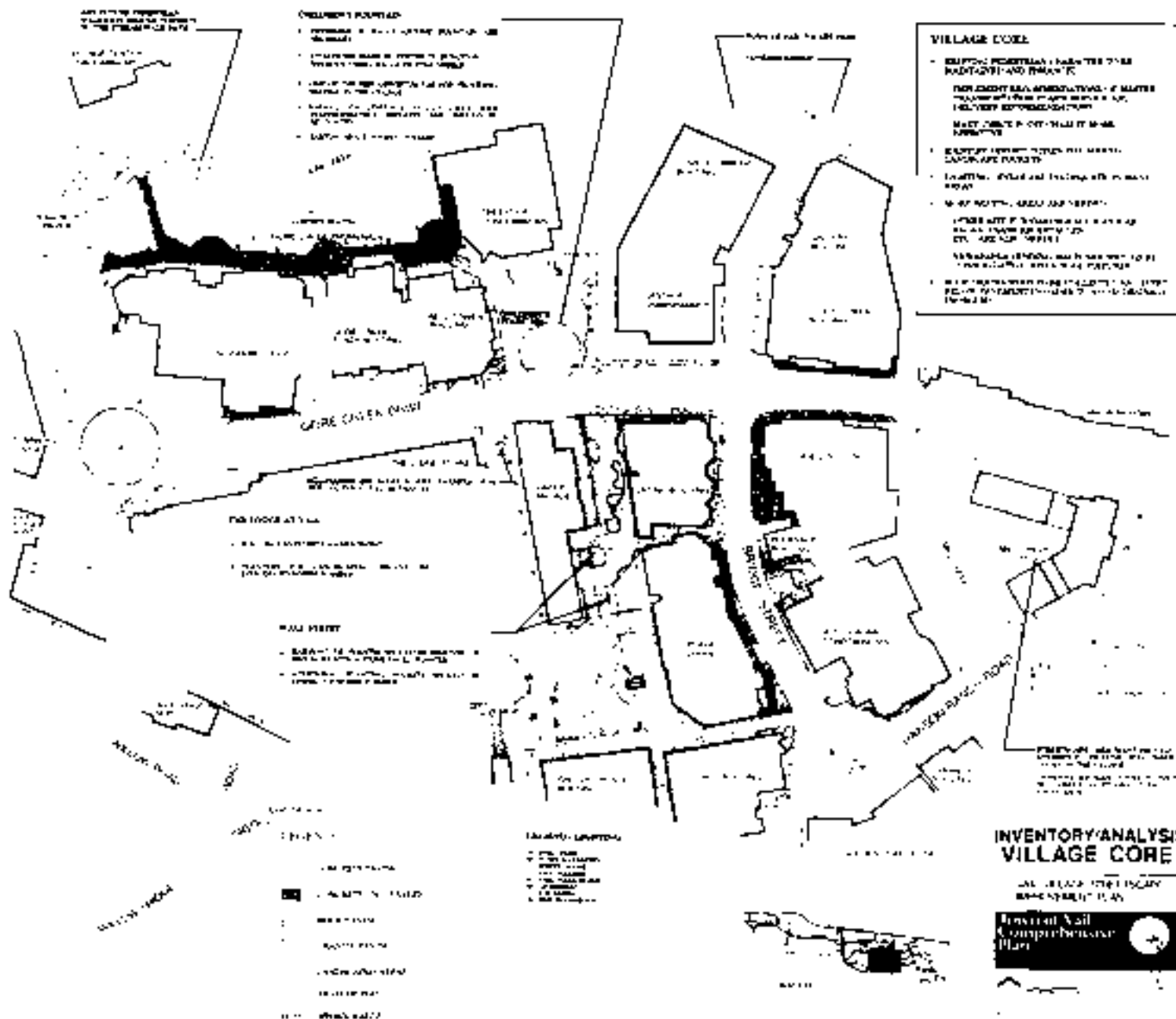


Figure 14

# Streetscape Improvement Plan

## *Village Core*

The focus of the proposed improvements for the Village Core is to enhance the existing streetscape framework. The framework for a remarkable streetscape setting is already in place in the Core area and the suggestions included in this Master Plan will add the detailing, that extra level of finish, that creates a statement of quality and a distinctive character. Because of the high quality of the existing framework, there are no major changes proposed for the Village Core. Except for a few key areas, the majority of the improvements relate to paving or adding site amenities.

### PRELIMINARY CONCEPTS

Many of the preliminary concepts for the Village Core dealt with the amount of detail the street paving should contain. Another area of concern was the question of just how much latitude should private properties have in selecting paving materials.

#### *Right-of-Way Paving Treatments*

Three levels of detailing were presented for the right-of-way paving treatments. These concepts ranged from the very simple, to a plan which showed intricate paving patterns. The public response to these concepts was fairly consistent. The majority of the public and Town staff felt that the street surface did not need to be heavily detailed and that the architecture and the storefronts were the focus in the Village Core. There was agreement that the streetscape treatment should form a backdrop for these elements. Most felt that more detailed paving treatments were appropriate for key intersections, focal points and features such as the Children's Fountain.

#### *Paving Treatments for Private Properties*

As there are few curbs in the Village Core, there is little or no distinction between the public right-of-way and private property. The result is that paving treatments on private properties, and those in the right-of-way are perceived as a unit, and should be compatible. The preliminary concepts for paving private properties in the Core area attempted to identify the range of paving materials that would be allowed, from requiring the same paving material that is used in the right-of-way to allowing any durable paving treatment. The primary issue became how to create compatibility while allowing a reasonable amount of flexibility for the individual property owner. In the end, there was agreement that for private

property, three to five paving materials, in a limited color range would provide adequate flexibility while maintaining continuity.

Other valid design suggestions for the Village Core, that were not included in the final plan included:

- Relocating Seibert Circle to a sun pocket adjacent to the Red Lion's deck. This idea, which is included in the Urban Design Guide Plan, was rejected in order to maintain fire access to upper Bridge Street and Wall Street.
- There was general agreement that moving Check Point Charlie out of the Core was a good idea. New information booth locations were proposed south of the intersection of Vail Road and Willow Road and at the top of the hill on Vail Road just east of the intersection with Forest Road.

## **PREFERRED STREETScape PLAN**

*Design Concept:* The design concept hopes to create an interesting network of plazas connected by walkways and streets that will enhance an already exciting walking experience. An effort has been made to further enhance the Village area from Willow Bridge to Hanson Ranch Road as a place for pedestrians. A simple but elegant paving treatment, additional landscaping, Ted Kindel Park and subtle design details (by the use of benches, lighting, etc.) are suggested to enrich and integrate an already successful pedestrian village. Please see the perspective sketch of Gore Creek Drive (Figure 16) and the preferred Streetscape Plan for the Village Core (Figure 15).

### *Pedestrian Circulation*

As the Village Core area is already a pedestrian zone, there are very few changes being suggested regarding pedestrian circulation. Those recommended changes include:

- Removing the loading and delivery zones from Gore Creek Drive and Bridge Street as recommended by the Master Transportation Plan.
- A pedestrian connection (between the Red Lion and the Rucksack Building) from Bridge Street over Mill Creek, to the Mill Creek Court Building, is called for in the Plan. This was a recommendation of the Urban Design Guide Plan and the Transportation Master Plan.

- During the proposed 1992 reconstruction of Willow Bridge, the adjacent pedestrian bridge (to the east) will be retained. Upon completion of the Willow Bridge reconstruction, pedestrian movements in this general area should be studied to determine if the pedestrian bridge should be retained and upgraded or removed.

### *Suggested Paving Treatments*

A key point to remember when discussing the suggested paving treatment in the Village is that it is not intended that all paving will occur at one time. This is a 20 to 30 year improvement plan. The plan suggests completing the focal points and plazas first, with perhaps a linear segment on Bridge Street to evaluate what impacts the paving will have on the character of the Village. Each plaza design should be further refined to insure the work will have the desired effect of enriching the public spaces. A monotonous sea of pavers or overly complicated paver patterns must be avoided.

In addition, the demarcation between the public right-of-way and private land may be appropriate to dissolve or emphasize, depending on the individual project site. The result will be to create a varied street color and texture that allows private property owners creativity, but also establishes a comprehensive design context to work within.

The primary paving material for the right-of-way area of the Village Core is recommended to be rectangular concrete unit pavers, in the color mix specified in the Guidelines for Paving section of this report. The herringbone pattern, which is proposed for most areas, is edged by a double soldier course. The intent is to satisfy the need for a simple streetscape treatment without being monotonous. The double soldier course also creates a point for starting and stopping pavers proposed by private developers that will be compatible with the overall phased paving design.

Focal points - such as the Children's Fountain, the intersection of Bridge Street and Gore Creek Drive and Seibert Circle - will receive special paving treatments.

A range of five paving materials has been designated for use on private property, one of which is the rectangular concrete unit paver suggested above for use in the public right-of-way.

### *Special Features*

A number of proposed improvements are unique to this sub-area. These include:

- Restoring the Children's Fountain to its original design by removing the stone walls to allow access to the water. This most likely will require reworking the water circulation system to install return drains which will not be easily plugged with debris. Consider remounting the existing, large sculptures on a more appropriate, concealed base.
- Check Point Charlie is recommended to be moved south, up the hill to the intersection of Vail and Willow Road (Figure 15). This will reduce the amount of vehicular traffic turning around in the Village Core by providing an exit point (Willow Road) for those vehicles that are not permitted to enter the Core area.
- Seibert Circle is proposed to be replaced by a large planter that is surrounded by low steps for informal seating. A large "specimen" evergreen tree should be maintained in the Circle. Seibert Circle's focal point could be a fountain or an artwork feature that will be visible from much of Bridge Street. One theme currently being discussed is a public art project honoring the 10th Mountain Division. Seibert Circle's ability to be used as a performance site should also be considered.
- Special paving is called for at the intersection of Gore Creek Drive and Bridge Street. This could also include a feature such as a compass rose, as a part of the paving pattern.
- Public restrooms are needed in the Village Core. As there are no viable locations to construct a new facility, it may be possible to incorporate a public restroom into an existing building. This will require the cooperation of a building owner in the Core but could possibly be done as a part of redevelopment project.

### *Landscaping and Site Amenities*

Where possible, planters and trees have been added. While there are significant restrictions due to fire access requirements, Gore Creek Drive and the south half of Bridge Street have been softened with additional landscaping.

- Along Gore Creek Drive in front of The Lodge at Vail, the Plan suggests removing as much of the existing rock wall that separates the sidewalk from

the street as possible, and replacing it with steps. With the relocation of the service and delivery spaces from this area, the need for the existing wall is eliminated. Planters should be integrated into the steps to soften the street edge. Care must be given to locating the planters so that they are compatible with the storefronts and signage. Heating the walkway and steps is also recommended. These improvements are illustrated in Figure 16.

- Unsightly alleys, trash compactors and enclosures, service/delivery entrances, etc., should be screened whenever possible. The use of native landscaping for screening is strongly recommended.
- Planters have been suggested at Seibert Circle and at the present Check Point Charlie location. The landscaping should be designed to allow for possible performances by street musicians or other artists.
- The plan calls for improvements to Ted Kindel Park. Landscaping, seating and additional lighting is desperately needed along Mill Creek. Improving the stream channel by bringing it back to its natural character is also recommended. This area also has the potential for a public performance area.
- The green space north of the Gore Creek Promenade should be regraded to allow for better drainage so people can sit on the lawn comfortably. Additional planting areas should be considered and improving the streambank is also recommended. This site also has the potential for a public art project. Adding groups of smooth, flat rocks set at seating height (+/- 18") will provide informal seating opportunities at the Creek's edge. Access to Gore Creek should be maintained and consideration should be given to the creation of a small "children's beach."
- Seasonal street trees, in removable planters with seating, have been proposed on the south side of Gore Creek Drive and on the east and west sides of Bridge Street.
- Where adequate room for permanent landscaping does not exist, seasonal, movable planters should be used. In addition, each new light pole installed should be equipped to handle hanging flower baskets.



MOVEABLE SEASONAL PLANTER

- Stairs connecting Bridge Street to the pocket park and Gore Creek on the north side of the Covered Bridge Building are needed. The pocket park should be improved so that it could function as a picnic area or performance site in the Village.
- Vail's colorful history should not be ignored. Each major plaza, fountain or park area should have a sign with its name, and if possible, a brief history of the origin of the name.

Potential locations for seating and clustered newspaper dispensers are shown on the Plan (Figure 15). The newspaper dispensers have been suggested at key points along the street. Because the Town will be primarily using movable, seasonal benches, different locations can be tried until the best are found. The Plan suggests a number of locations that appear to be good sites. Trash receptacles should be paired with benches when possible, especially where there are groups of benches. It is recommended that two or three benches be added to the Willow Road/Willow Place park.

Lighting is addressed in the Guidelines for Paving, Public Art, Site Furnishings and Lighting section of the report.



**COSTS**

The following cost estimate does not include improvements to private properties. The cost of improving underground utilities is also not included in the streetscape improvements.

Demo asphalt	63,900 sf x \$4.50/sf	\$287,500
Demo softsurface	9,000 sf x \$1.25/sf	\$1,100
Concrete unit pavers	63,100 sf x \$6.00/sf	\$378,600
Lighting	10 x \$2,500	\$25,000
Benches	10 x \$600	\$6,000
Trash receptacles	5 x \$450	\$2,250
Newspaper dispensers (6 per cluster)	2 x \$2,500	\$5,000
Signage	Lump Sum	\$3,000
Deciduous trees (includes irrigation)	93 x \$300	\$27,900
Coniferous trees (includes irrigation)	62 x \$350	\$21,700
Shrub beds and irrigation	1,050 sf x \$3.00/sf	\$3,100
New control gate	Lump Sum	\$15,000
Move Checkpoint Charlie	Lump Sum	\$30,000
Subtotal		<u>\$906,150</u>
Relocate utilities	10%	\$90,615
		<u>\$996,765</u>
Miscellaneous contingency	15%	\$149,514
		<u>\$1,146,279</u>
Design & construction management	20%	\$229,255
<b>Total</b>		<u><u>\$1,223,784</u></u>

Note: It is essential that maintenance budgets be increased, especially snow plowing, to cover the cost of adequately maintaining the new streetscape improvements.

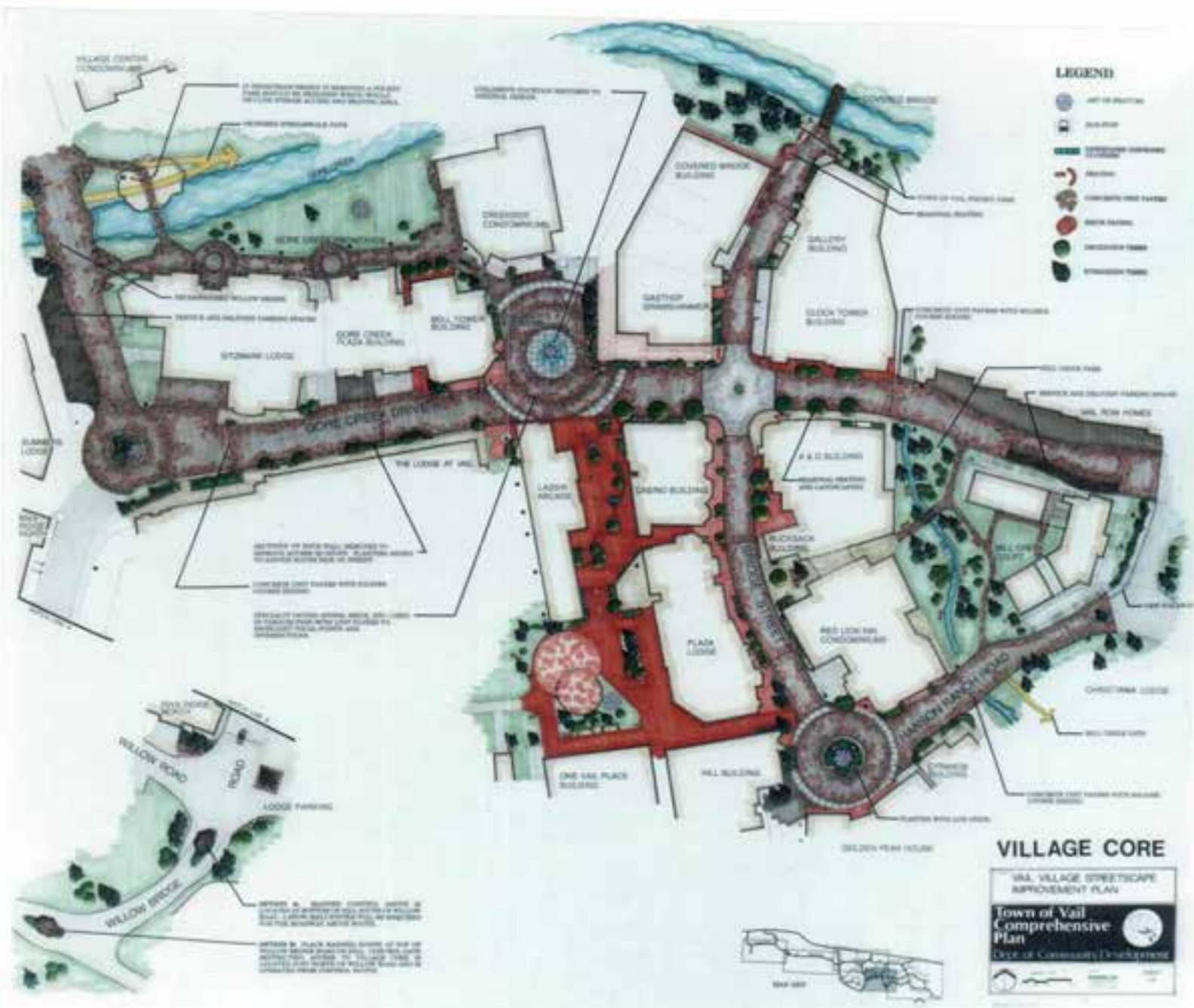


Figure 15



Figure 16

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## EAST VILLAGE

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### Existing Conditions

For the purposes of this Master Plan, the general area east of Mill Creek to the Town soccer field is referred to as the "East Village". This area includes Vail Valley Drive, Hanson Ranch Road and East Gore Creek Drive. Because of the larger scale of the area, a 1"= 50' air photo has been used as the base map. (All other base maps for the Master Plan are 1"= 20'.) A graphic summary of



the Inventory and Analysis of the East Village sub-area is shown in Figure 17.

### CHARACTER

This sub-area is predominantly made up of lodges and condominiums. Vehicular traffic is not restricted. The Golden Peak ski base facility and the Children's Center are at the center of the sub-area and are both major vehicular and pedestrian destinations. Accommodating pedestrian movement, while maintaining vehicular flow, will be one of the challenges in this portion of the study area. Clearly, the East Village must continue to allow auto access while separating pedestrians from auto traffic.

The existing streetscape character is generally open, although several buildings are close to the right-of-way. The primary impression of the East Village, however, is one of large parking areas adjacent to the right-of-way. This is

especially true in the west half of the area. There are a few small landscape islands along the parking lots within the center of the sub-area and the primary landscape relief occurs where Vail Valley Drive approaches the soccer field at the east end of the sub-area. Mature aspens and evergreens are located along Vail Valley Drive, mostly on private properties.

There have been individual attempts to provide separate pedestrian ways in the East Village. The resulting sidewalk system is too scattered and not continuous to be very effective. The primary goal for this area is to provide an effective and safe pedestrian system, while accommodating vehicle and bicycle traffic.

## CIRCULATION

### *Vehicular*

There are no restrictions to vehicular traffic on Vail Valley Drive, while Hanson Ranch Road and East Gore Creek Drive are one-way streets. There are numerous intersections and curb cuts along most of the roads in the sub-area; and several instances of head-in parking areas that would require vehicles to back directly into the right-of-way.

Traffic along Vail Valley Drive is very heavy throughout most of the year. It is especially heavy in the morning and late afternoons during the ski season, and evenings and weekends during the summer months. Vail Associate's Children's Center is a major source of vehicular congestion during the ski season with parents dropping off and picking up children at the beginning and end of the day. This congestion has caused the Town to consider relocating the Town bus turnaround away from the Children's Center and to the north.

Traffic on Hanson Ranch Road and West Gore Creek Drive is not as heavy as on Vail Valley Drive; however, there is still congestion caused by skier drop-off, parking for deliveries and lodge parking along these roads.

Because of the narrow right-of-way on the roads in the sub-area, the extent of the streetscape improvements will be limited. This situation is further complicated because the existing roadway is often not aligned with the right-of-way. This has resulted in private improvements being constructed in or right up to the edge of the right-of-way.

## *Pedestrian*

Because there are no continuous sidewalks in the East Village sub-area, pedestrians primarily walk on the shoulder of the road. On Hanson Ranch Road and West Gore Creek Drive, this shared use of the roadway by pedestrians is not a problem. However, on Vail Valley Drive the heavier traffic and greater speed will require the separation of pedestrians and vehicles. In addition, there is likely to be an increase in pedestrian traffic along Vail Valley Drive because of the expansion of the Village parking structure and the creation of a new exit portal from the parking structure at Vail Valley Drive.

One of the Town's asphalt recreational paths runs from the Vista-Bahn lift to the Gold Peak ski base facility. It connects to Vail Valley Drive east of the Children's Center. There is also a connection to Hanson Ranch Road. This path is heavily used during both the winter and summer months.

## **PAVING/DRAINAGE**

The roadways throughout the East Village area are asphalt and will remain so in the final Master Plan.

There is currently a 5' wide, concrete sidewalk along the north side of Vail Valley Drive adjacent to the Rama-Horn Lodge. The walkway then extends through the Manor Vail property and becomes wider, a portion of which is constructed of concrete unit pavers. At its easterly end, it connects to a covered bridge that accesses Ford Park.

Drainage improvements are also scheduled for Vail Valley Drive. One of the main problems to be addressed will be to intercept storm flows coming off the mountain at Gold Peak and the Children's Center. For the most part though, the streetscape improvements that will occur in this sub-area will almost certainly be adjacent to the roadway. In addition, there is clear direction from Staff that concrete curb and gutter will be needed on Vail Valley Drive. This will be an appropriate treatment given the existing conditions and pedestrian and vehicular traffic in the area.

## **SITE AMENITIES**

Most of the site furnishing in the East Village area are on private property at the Gold Peak ski base facility and at the Children's Center. There is one small, wood, bus shelter with seating, just east of the Children's Center.

There is a need for additional seating opportunities along the pedestrian system in the East Village area.

Roger Staub Park, on East Gore Creek Drive, is an existing amenity and potential destination in this sub-area. This is an excellent example of a pocket park. It is under used due to a lack of signage to direct people to the park and because the park appears to be private property.

## LANDSCAPING

The East Village is well-landscaped with mature trees and landscape planters. The major problems are the use of railroad ties for planters in parking lots, and parking lots on Vail Valley Drive that have little or no setback or screen landscaping.

Landscape treatments, that might be proposed as a part of the Master Plan, will only address those areas where landscaping infill is needed.

## UTILITIES

Since the streets will not receive specialty paving, there should be no conflict between utility lines and improvements that will be proposed by the Master Plan. Above-ground utility pedestals, light poles, etc., at the edge of the right-of-way will have to be incorporated within the proposed pedestrian walkways.

### *Lighting*

The "Town and Country" light fixture is used in the East Village area, primarily at intersections. The spacing of the lights averages 300' apart. However, there are some sections, such as Vail Valley Drive south of Manor Vail, that have no lights at all. While there were only a few public comments regarding inadequate lighting in the East Village area, it is safe to assume that some additional lighting will be required.

## PUBLIC COMMENT

Residents of the East Village sub-area had a wide range of opinions on the types of streetscape improvements desired in the area. Most of the comments received focused on the Blue Cow Chute to Mill Creek Circle portion of Vail Valley Drive. The primary areas of concern were:

- The heavy vehicular and bus traffic on Vail Valley Drive -- both in the winter and summer seasons. At peak periods, vehicular traffic is "bumper-to-bumper". In the summer, the mix of buses, pedestrians, bicycles and cars in the roadway creates conflicts. In general, it was felt that uses should be separated as much as possible.
- The existing 4'-5' wide walks in the area are too narrow to handle the level of pedestrian traffic that passes through the area.
- There was general agreement that most of the pedestrians prefer to use the west side of Vail Valley Drive.
- Screening of cars is needed for the Vail Associates' parking lot. For the most part, it was felt that parking should not be allowed immediately adjacent to any of the streets.
- Comments were received both for and against maintaining head-in parking directly adjacent to a street. The key concern was how to design pedestrian walkways behind parked cars and still maintain the required parking.
- The existing bus stops create vehicular choke points in the Garden of the Gods' area along Vail Valley Drive.
- Many felt that both existing bus stops on Vail Valley Drive may not be necessary or that they should be relocated. Suggestions were made to move the bus stops near Mill Creek Circle, or to the new portal at the east end of the Village parking structure.
- Some felt that additional lighting was needed, but that avoiding light spillover into the residential areas would be essential.
- It was noted that the west end of Hanson Ranch Road is heavily used for loading and delivery and the street is also used as a drop-off point for skiers.



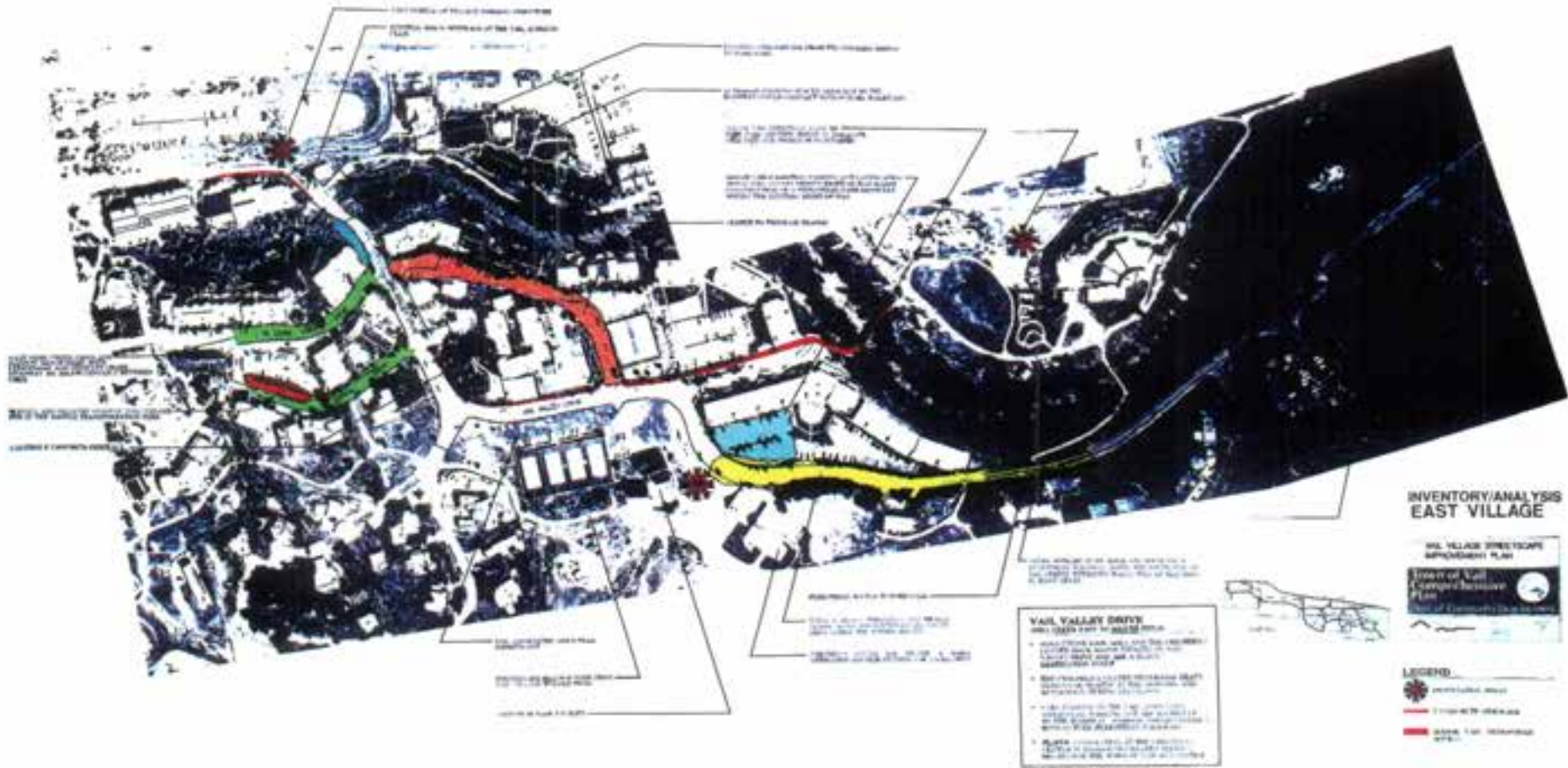


Figure 17

# Streetscape Improvement Plan

## *East Village*

The Streetscape Improvement Plan (Figure 18) for the East Village focuses on the pedestrian corridors adjacent to the asphalt roadway. The primary goals are to create a safe environment for the pedestrian along Vail Valley Drive while accommodating the heavy vehicular traffic, and directing pedestrians to key destination points, such as the Gold Peak ski base facility, the Children's Center, Ford Park and the Ford Amphitheater and the Village Core.

For Hanson Ranch Road and East Gore Creek Drive the goal is to maintain the present character but to also improve such elements as lighting, landscaping and roadway edge treatments.

No major changes to the vehicular circulation system or the roadway are proposed as a part of the Plan for this sub-area.

## PRELIMINARY CONCEPTS

Of all the sub-areas covered in this Master Plan, the East Village has remained the most consistent with the concepts that were originally proposed for the area. The concept of creating separate pedestrian corridors that are adjacent to the roadway was proposed, and adopted, very early on in the design process.

Some of the other ideas and concepts that were suggested and discussed during this planning process included:

- Removing both the east and west-bound bus stops near the intersection of Vail Valley Drive and West Gore Creek Drive. Some of the suggestions for implementing this concept included moving the stops to the east portal of the Village parking structure and/or to the intersection Mill Creek Circle and Vail Valley Drive.

The idea of moving the westbound bus stop to the parking structure was discarded because the Town's Transit Department felt it would be too close to the bus stop at Slifer Square. Moving the eastbound bus stop to the

intersection of Mill Creek Circle and Vail Valley Drive was discarded as it was believed to be too close to the Gold Peak bus stop.

- It was suggested that the pedestrians going from the Village parking structure to Gold Peak could use the vacated street right-of-way, to the north and east of the Rams-Horn lodge (presently used for private parking.) This would avoid much of the pedestrian/vehicle conflict that now exists on Vail Valley Drive.

This idea was thought to be unfeasible due to the problems with re-establishing the public right-of-way and the probability of losing parking spaces.

- Creating a pocket park with an ice skating rink, west of Vail Associates' Day Lot at Gold Peak was also mentioned. While it was agreed that this was generally a good idea, it was felt it was beyond the scope of this Plan.
- Adding bike lanes to the roadway was also felt to be a good idea but not practical due to the narrow public right-of-way along Vail Valley Drive.
- There were a few suggestions for major changes to the vehicular circulation patterns on Vail Valley Drive, such as making the road one-way eastbound and adding a bridge over Gore Creek, just east of the Nature Center. This would create room for bike lanes and possibly separate bus lanes and pedestrian paths. However, revisions to the existing circulation patterns at this scale, while promising, would require major transportation studies to determine their potential impact.
- It was generally agreed that private improvements, such as parking areas which encroach into the public right-of-way should be removed.

## **PREFERRED STREETSCAPE PLAN**

The proposed improvements for Vail Valley Drive are illustrated on Figures 18, 19 and 20. Because the problems here are similar to those found on West Meadow Drive, the streetscape treatments are similar as well. Unfortunately, there are fewer opportunities for amenities here because of the narrow public right-of-way.

Due to the scale of this sub-area and the straight forward nature of the improvements; the Plan for the East Village (Figure 18) is diagrammatic. A detail of the Blue Cow Chute to Mill Creek Circle portion of Vail Valley Drive (Figure 19) is provided to better illustrate the proposed improvements for this sub-area.

### *The Design Concept*

There was clear public consensus that asphalt roadways with wide sidewalks, separated from the street by curb and gutter, were appropriate on Vail Valley Drive. There was also general agreement that where there was only room for a wide sidewalk on one side of the street, the west and south sides would be best suited. Pedestrian ways on the west and south sides provide good access to the major destinations in the area - the Village Core, Gold Peak and the Children's Center. The proposed improvements are also intended to highlight and improve access to Ford Park and the Nature Center. Improving vehicular circulation, by removing pedestrians from the street will be an additional benefit of these proposed changes.

For Hanson Ranch Road and East Gore Creek Drive, the concept of the pedestrians and vehicles sharing the same roadway was generally endorsed as a workable solution, given the very narrow right-of-way and lower traffic volume. Throughout this area, more landscaping is necessary to soften the building facades and to screen surface parking lots.

### *Pedestrian Circulation*

The proposed streetscape improvements for the East Village sub-area are as follows:

- Vail Valley Drive (from Blue Cow Chute to the entry to Manor Vail)
  - West and South Sides: Concrete unit paver walkway, 8' - 10' wide
  - East and North Sides: Concrete unit paver sidewalk, 5' - 6' wide

At this time, the primary pedestrian path is proposed to go on the south side of Vail Valley Drive, adjacent to Vail Associates' Day Lot. Having parked cars immediately adjacent to a major walkway is less than desirable, however, the path is proposed in this location with the intent that, as the site redevelops, allowances will be made for pedestrians.

- Vail Valley Drive (from Manor Vail's north entry to the soccer field)
  - South Side: Concrete unit paver walkway, 8' - 10' wide
  - Retaining walls will be needed in some areas due to an inadequate right-of-way and a hillside being immediately adjacent to the roadway.
  - There is no sidewalk proposed for the north side of the street due to the tight physical constraints along the northerly right-of-way line.
  - A sidewalk/crosswalk solution will be necessary to provide safe pedestrian access through the Golden Peak bus turnaround area.
- The existing Vista Bahn/Gold Peak recreational trail and the pedestrian connection to Ford Park through Manor Vail, will be used as a part of this sub-area's pedestrian system. Manor Vail's entry to Ford Park should include additional signing or an entry statement.
- Additional minor pedestrian paths, either concrete or asphalt, are proposed as follows:
  - An east/west connection between Vail Valley Drive and the existing recreational trail west of the Tivoli.
  - A path north of the tennis courts to the north entry of the Gold Peak ski base facility.
- The existing shared use of the street, by pedestrians and vehicles on Hanson Ranch Road and East Gore Creek Drive will continue. Brick or concrete unit paver bands should be added at the edge of the asphalt to better define the roadway.
- Consider eliminating the winter-time parking use on Chalet Road. Closure of the dead-end road and development of a pocket park/open space area should be pursued.

### *Special Features*

The proposed improvements for the East Village sub-area include a number of unique features in addition to walkways and paths:

- Two small landscaped plazas are proposed at the south entry to Ford Park and at the point where Vail Valley Drive turns to the east at Mill Creek Circle. In each case the intent is to create a focal point that accents these locations.
  - At the Ford Park entrance, a handicap drop-off is needed in addition to concrete unit pavers, seating and landscaping. These improvements will increase the visibility and usefulness of this important access to Ford Park and the Nature Center.
  - A small plaza with landscaping at the Vail Valley Drive/Mill Creek Circle intersection is intended to provide a "pivot point" for pedestrians moving between the Village parking structure and the Gold Peak ski base facility. Currently there is no visual connection between these two facilities. In order to discourage unnecessary vehicular traffic on Mill Creek Circle, consideration should be given to narrowing the intersection with Vail Valley Drive.
- It is proposed that the Children's Center bus stop be relocated to the northeast of the Gold Peak ski base facility for two reasons: 1) congestion caused by heavy drop-off traffic at the present location causes delays for buses and; 2) the proposed location provides better access for summer events in Ford Park. A pedestrian crosswalk from the relocated bus stop to the Manor Vail/Ford Park path will be necessary.
- The westbound bus stop, that is presently located on Vail Valley Drive near the bridge over Gore Creek, has been eliminated at the request of area residents and to reduce vehicular/bus congestion. The eastbound stop at the Garden of the Gods will remain.

### *Landscaping and Site Amenities*

Landscaping and site furnishings should be included where possible as the streetscape improvements are made. All bus stops and feature areas should provide a full compliment of site furnishings.

Additional landscaping is needed around the existing parking lot between Hanson Ranch Road and East Gore Creek Drive.

Roger Staub Park also provides an excellent seating opportunity, however better signage and lighting is needed to direct pedestrians to the park. Removal of a portion of the existing fence along East Gore Creek Drive is recommended and, overall, a "public park" image should be attained. Additional picnic tables should be added.

Lighting and utilities will be covered in the Guidelines for Paving, Public Art, Site Furnishings and Lighting section of this report.

### *Implementing the Concept*

Figure 19 is a detail of Vail Valley Drive from the bridge over Gore Creek south to Mill Creek Circle. The detail illustrates:

- The major and minor pedestrian paths on each side of the street;
- The integration of a bus stop into the Garden of the Gods' site;
- Additional landscaping and the proposed focal points;
- Reconfiguring the parking at the Vorlauffer to provide for a pedestrian walkway on the west side of Vail Valley Drive. Of the 12 existing spaces, two "guest" parking spaces for the Vorlauffer may need to be relocated to the east side of Vail Valley Drive. The final design shall ensure that there is no net loss of parking spaces for the Vorlauffer;
- Relocation of parking and planters on the east side of Vail Valley Drive; and
- Widening the Vail Valley Drive bridge over Gore Creek to better accommodate the proposed pedestrian walkways,
- Adding a neck down at the east end of Hanson Ranch Road (at Vail Valley Drive). This narrowing of the roadway discourages unnecessary traffic and provides an opportunity for additional landscaping.

**COSTS**

While the improvements recommended for Vail Valley Drive are not as extensive as other sub-areas, the length of the corridor results in higher overall costs. This estimate does not include resurfacing the roadways in the sub-area.

Demolition

Asphalt	15,400 SF X \$4.50/SF	\$69,300
Curb and gutter	Lump Sum	\$6,100
RR tie planters	4 x \$1,200	\$4,800

Construction

Grading	19,500 SF x \$3.00/SF	\$58,500
Curb and gutter	3,120 LF x \$13.00/LF	\$40,600
Concrete walk	2,500 SF x \$3.00/SF	\$7,500
Concrete unit pavers	21,850 SF x \$6.00/SF	\$131,050
Asphalt patching	10,300 SF x \$1.50/SF	\$15,400
Retaining walls	7,660 Face Feet x \$15/FF	\$114,900
Widen bridge deck	Lump Sum	\$50,000
Parking Lot planters (stone)	1400 Face Feet x \$50/FF	\$70,000
Misc. landscape plants	Lump Sum	\$30,000
Misc. Site Furnishings and Signage	Lump Sum	\$14,000
Lighting	13 x \$2500	\$32,500

Subtotal		<u>\$644,650</u>
Relocate utilities	10%	<u>\$64,465</u>
		\$709,115
Miscellaneous Contingency	15%	<u>\$106,367</u>
		\$815,482
Design & construction management	20%	<u>\$163,096</u>
<b>TOTAL</b>		<u><b>\$978,578</b></u>

Note: It is essential that maintenance budgets be increased, especially snow plowing, to cover the cost of adequately maintaining the new streetscape improvements.



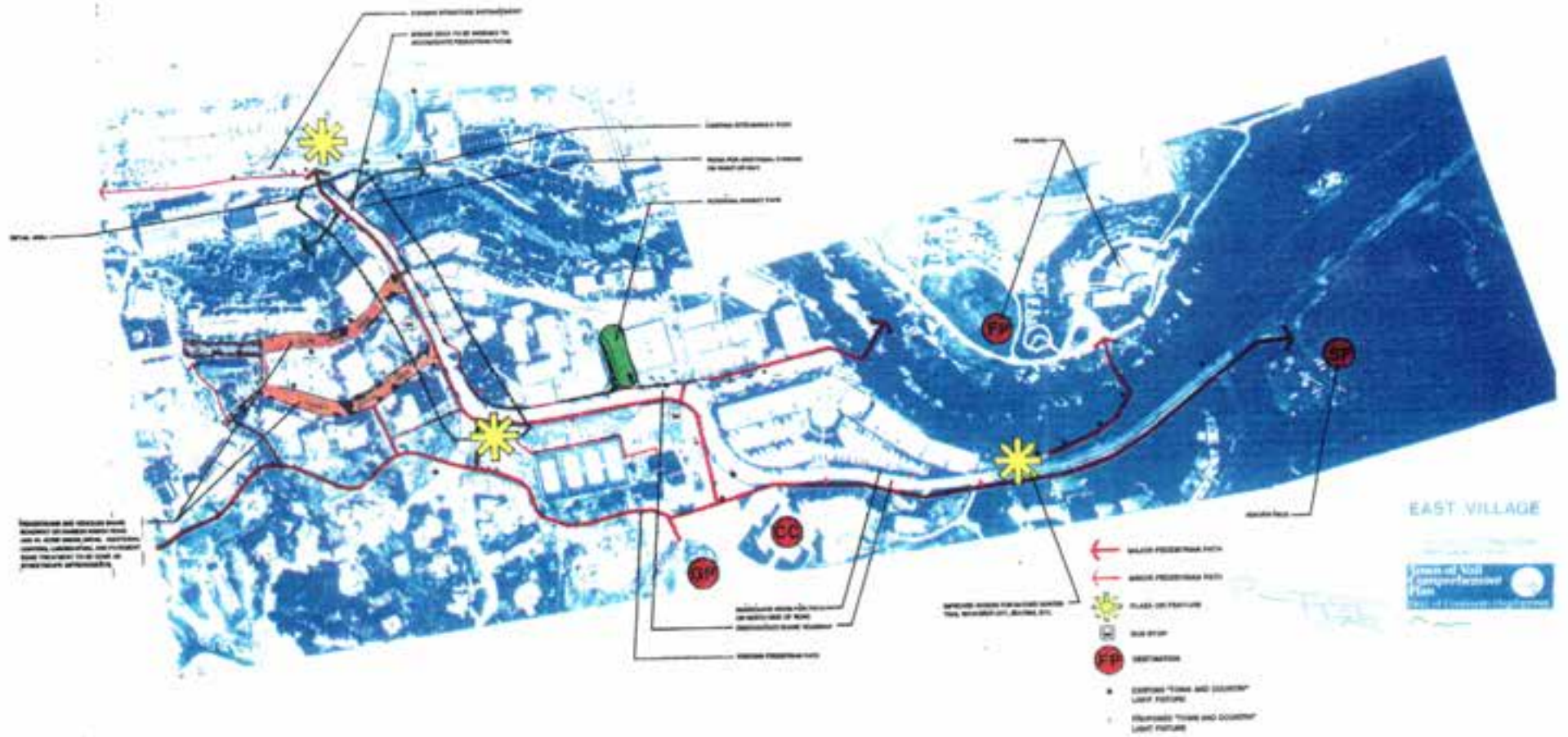


Figure 18

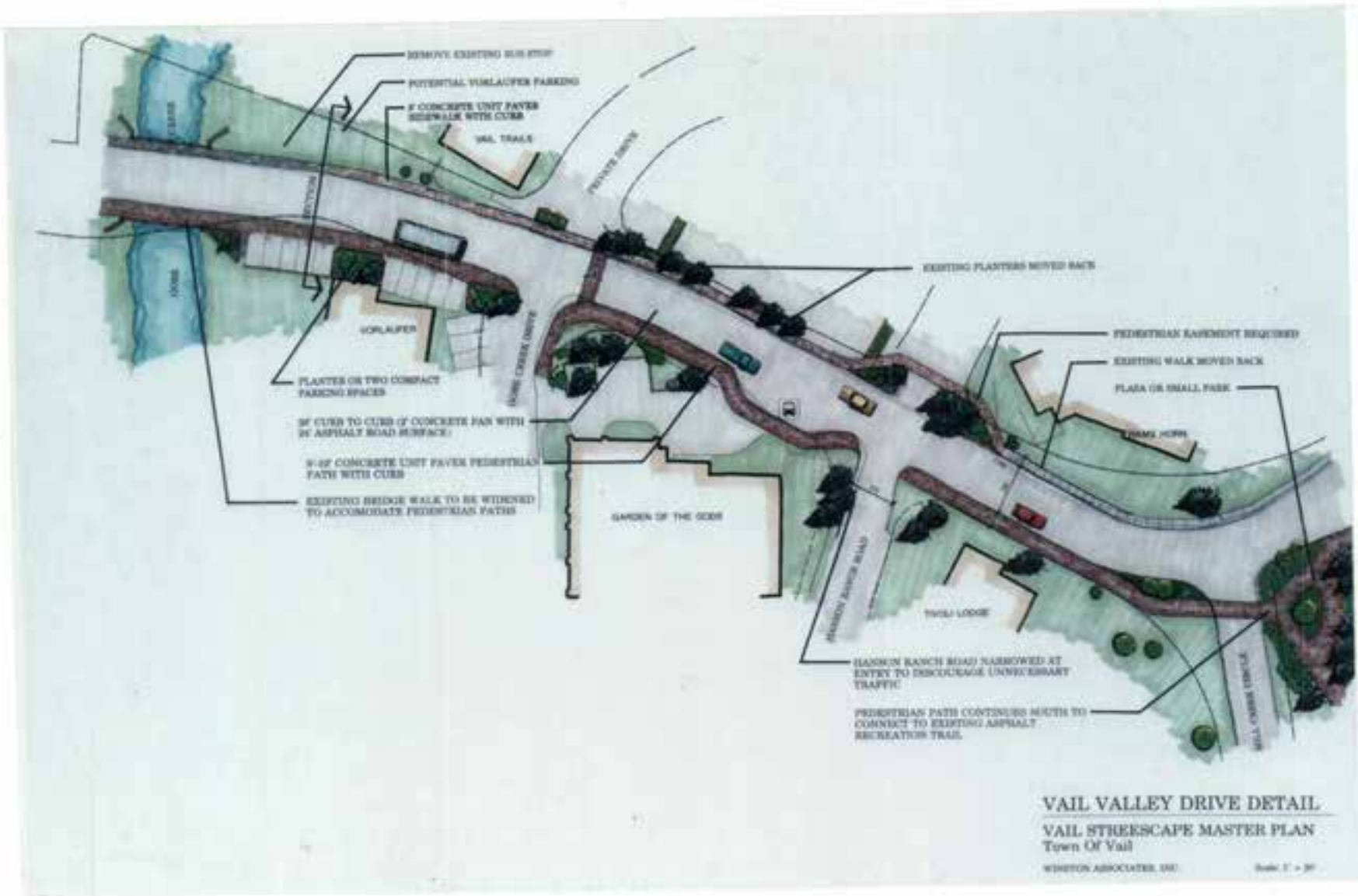


Figure 19

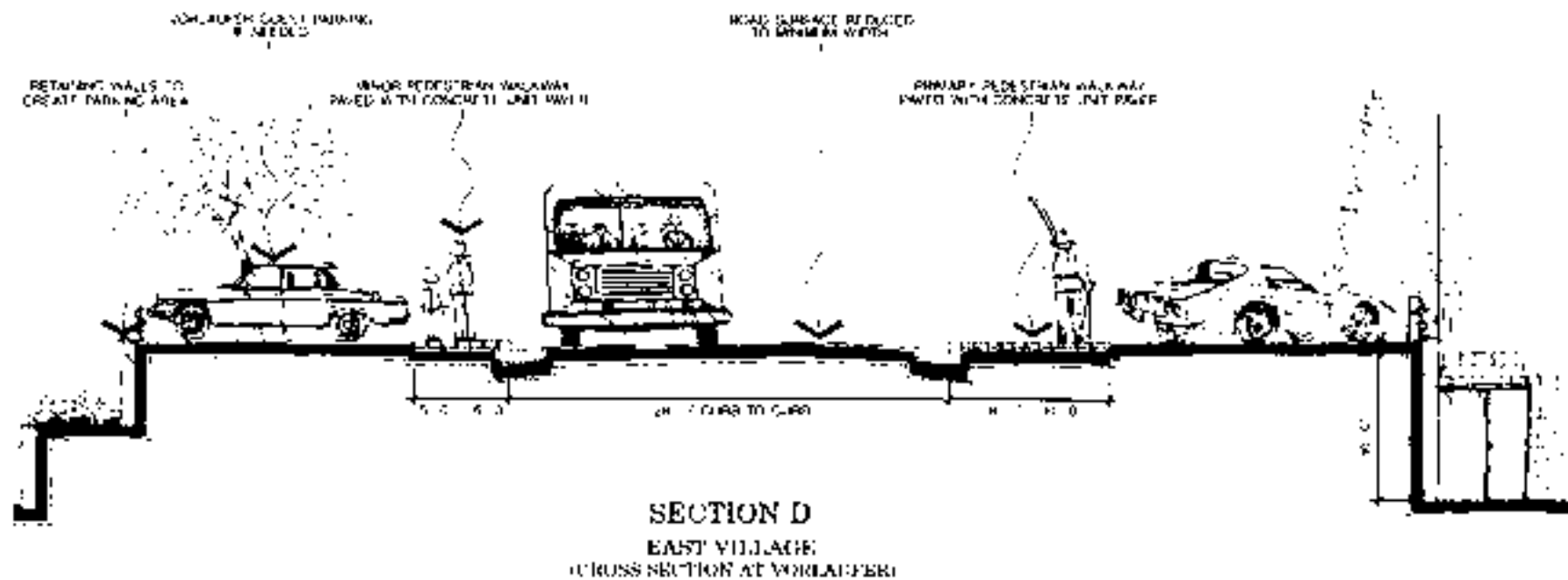


Figure 20

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# **GUIDELINES FOR PAVING, PUBLIC ART, SITE FURNISHINGS AND LIGHTING**

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## **Objectives**

The intent of this section of the Streetscape Master Plan is to establish guidelines for paving materials, locations for public art, site amenities and lighting that will be used to implement the recommendations of the Master Plan.

The guidelines that follow will strive to achieve these objectives:

- To establish a set of standards that will allow the Town to implement the Streetscape Master Plan in phases and still achieve a cohesive, quality streetscape treatment (i.e., sidewalks, curb/gutter sections, walls, etc.)
- To specify materials that will be durable and of a high quality, in order to reduce long term maintenance costs.
- To establish a range of paving materials and colors which will be used in the public right-of-way.
- To establish a menu of paving materials and colors that will be acceptable for use on private property, adjacent to the right-of-way.
- To specify design concepts for seating, trash receptacles, bike racks, etc., for use in the Master Plan area.
- To establish a conceptual plan for lighting and to specify light fixtures.
- To identify sites which are appropriate for the development of site specific public art projects and the placement of gifted art pieces.

- To comply with the standards for handicap access set forth in the Americans with Disabilities Act.

Even though these guidelines are an essential step in improving Vail's Streetscape environment, they are certainly not the last step in the process. This Plan does not provide the level of detailed design that is needed for construction. Engineering, detailed design and construction documentation must be completed before the recommendations contained in this document can be implemented.

## Paving Systems

Vail's mountain setting produces climatic conditions that greatly shorten the life of many typical paving materials. This has resulted in many different paving systems and installation techniques being attempted - with mixed results. It has become obvious that some paving systems, such as concrete, do not hold up to Vail's alpine climate. Other types of paving materials, such as asphalt, do not fit in with Vail's pedestrian character and distinction as a world class resort. The recommendations that follow suggest paving materials that are not only durable but are also compatible with Vail's character.

### PAVING IN THE RIGHT-OF-WAY

There is a wide range of paving materials which are suitable for Vail. These include:

- Concrete unit pavers on a gravel/sand sub-base
- Full depth brick pavers on a concrete sub-base
- Natural stone on a concrete slab (sandstone, granite, etc.)
- Some types of stone-composite pavers



Concrete unit pavers are proposed as the primary paving unit because they are more in keeping with Vail's character than concrete or asphalt, and have a much longer life span. They can be produced in Colorado, installed without a concrete sub-base and have proven durable in Vail. Concrete unit pavers can be easily

removed to allow for subgrade repairs and can be replaced immediately after the work is finished.

The primary paving unit for the Master Plan area is recommended to be the 4" x 8" x 3 1/2" (+/-) rectangular paver (the dimensions may vary with manufacturer). The pavers are typically set on a sand/gravel base. The design for the sub-base will vary with soils type and the type of vehicle that will be using the roadway. Detailed soils testing and roadway engineering will be necessary to determine the appropriate roadway section for each area as it is improved.

Streets that will be handling bus or truck traffic on a continuous basis will require, at a minimum, a geotextile under a high capacity gravel sub-base or a concrete slab as a sub-base. Once again, the final design for this type of roadway will depend on the existing soils.

The paving pattern for the pedestrian streets in the Village Core and for East Meadow Drive will be as shown in Figure 15. The primary field will be a basic herringbone design with a double soldier course at the edges of the right-of-way, and regular, perpendicular bands along the length of the street. The bands and soldier course will allow for paving to be phased by creating potential beginning/end for the paving system. The soldier course will also serve to separate the public right-of-way paving from the range of materials allowed on private property. At times, the distinction between public and private land should not be delineated if good design is better served by blending the boundary. Please see the comments relating to paving design in the Village Core sub-area.

During the public review of the proposed streetscape options, there was consensus that the paving treatments in the Village area should be simple rather than intricate. Therefore, a simple paving pattern is proposed, one which would not compete with Vail's unique architecture and that can be constructed in phases.

## PROPOSED COLOR RANGE

Recommendations for a specified color mix are being made to establish a starting point for the final detailed design. The actual color mix may change once a final design is submitted and reviewed. The range of tints for concrete unit pavers is almost unlimited. Given the scale of this project, special "Vail Blends" could be produced specifically for the Town. This would give the Town complete flexibility to develop a

color mix that fits its needs. Given a phased construction, there is a real opportunity to perfect the color mix before large scale installations are done.

The design also calls for using a certain color blend of pavers to identify pedestrian only areas and bus lanes. This approach creates a subtle means of identifying how different public spaces are being used. It is hoped that this approach will help to make pedestrians feel more at ease in mixed use (traffic) areas.

For the primary commercial areas, the Village Core and East Meadow Drive, the proposed color range is a charcoal grey with a reddish/black accent. The mix listed below utilized Claylite Inc.'s colors (a Denver manufacturer of concrete unit pavers). Other manufacturers, local and out-of-state, can also produce similar colors.

The proposed mix for the pedestrian areas is as follows:

Charcoal 50%  
Arapahoe Blend 30%  
Brown 20%

The soldier course banding is proposed to be entirely in Arapahoe blend pavers.

For bus lanes and services and delivery areas, the color mix should be darker so that tire marks and oil will not be as apparent. The herringbone pattern is also recommended in these areas. The recommended color mix is:

Charcoal 70%  
Arapahoe Blend 20%  
Brown 10%

Generally, the use of a curb and gutter section is discouraged. If, for safety or drainage reasons, it becomes necessary to utilize a curb and gutter section, it should be designed in a way which reflects the alpine character of Vail, and not done in what has been called a "suburban style". Tinted concrete curbing (charcoal) is recommended and perhaps granite curbing would be appropriate in some of the sub-areas. It is also recommended that a layer of asphalt be placed over the gutter section (when asphalt is used for surfacing the roadway) to further conceal the concrete gutter section.



**CONCRETE CURB AND GUTTER  
WITH ASPHALT OVERLAY**



**VERTICAL GRANITE CURB**

## ACCENT PAVING

While there was public consensus that the paving design should not compete with the Village setting, there was also concern that the paving treatments not become monotonous. The color blend which is proposed for the primary paver will provide a rich foundation for the streetscape character. However, a limited amount of accent paving will be needed to add interest to the street surface.

There are a number of key areas in the Village that, because of their importance or uniqueness, are proposed to receive special paving treatments. This Master Plan has designated the following areas for special paving treatment:

- The Children's Fountain
- Seibert Circle
- Slifer Square
- The intersection of Bridge Street and Gore Creek Drive
- The intersection of East Meadow Drive and Willow Bridge Road
- The bus stops at the Vail Village Inn, on East Meadow Drive.
- Lionshead bus turnaround
- Vail Road/West Meadow Drive intersection

It is best to refer to the individual sub-area plans to see how the paving design for these focal points is handled. The range of the accent paving materials include:

- Granite paving blocks: Either in pre-assembled granite sets or individual paving units.
- Sandstone paving: Sandstone can be cut into a variety of shapes and sizes and is not limited to the irregular or square "slabs" that are most often seen.
- Other shapes of concrete unit pavers: The industry is now producing a wide variety of shapes and sizes. In order to maintain continuity, only the rectangular paver, the scored rectangular paver, the square paver and the "cobble-style" paver are recommended.
- Full-thickness brick pavers: Rectangular or square brick pavers, which must be set on a concrete sub-base.





All of these accent materials have the required durability but are proposed sparingly due to higher per unit costs and to achieve the objective of keeping the pavement design simple.

## PAVING ON PRIVATE PROPERTY

One of the primary areas of concern has been the question of how should paving on private properties, that are adjacent to the right-of-way, be integrated into the streetscape design. This particular problem is limited to areas of Vail where private properties front directly on, or very near the right-of-way, such as the Village Core or East Meadow Drive. The initial paving design studies for these areas showed a range of treatments - from no limit on the type and color of paving allowed; to requiring private property owners to use only those materials used in the right-of-way. When these concepts were presented at the initial public meetings, it was obvious that the majority of the public felt that the property owner should have some flexibility in how to treat the streetscape which is adjacent to the right-of-way, but that a certain amount of control was desirable, and necessary.

In response to this, a set of recommended paving materials and colors has been developed for the Streetscape Plan. Because of the range of materials and colors allowed, the private property owner will have more than enough flexibility when paving areas adjacent to the right-of way. The approved paving materials include:

### *Concrete Unit Pavers*



Three basic shapes of concrete unit pavers are proposed: rectangular, square (4" and 8" square) and a mixture of square and rectangular shapes that give the pavers a look of random flag stones. While there are many other shapes of concrete unit pavers currently being produced, it was felt that the three basic shapes listed above were most in keeping with the character of Vail. The concrete unit paver industry is still evolving and new products are constantly being produced. It will be important for the Town to reevaluate the list of approved pavers from time to time to respond to new products.

There are variations of textures and finishes which would be allowed for each of the shapes listed above. The most common is to add a score joint to the basic rectangular paver, to make it appear to be a 4" square unit or "bush hammer", giving the surface an unpolished "granite" look. Other treatments may be developed that may also be appropriate, but these would need to be reviewed and approved on a case-by-case basis by the Design Review Board.

### *Natural Stone*

Thermal finish granite or sandstone would also be acceptable paving materials on private properties. These materials can be cut into a variety of shapes, but square or rectangular shapes would be preferred. It is important that these materials be laid on a properly designed sub-base. Concrete setting beds are strongly encouraged due to the presence of truck traffic in most areas. Sand/gravel setting beds should only be used where no vehicular traffic is anticipated or if it is specifically recommended by the manufacturer.

### *Brick Pavers*

Rectangular or square, full-thickness brick pavers set on a concrete sub-base are acceptable. Because of the wide range of colors available in brick, it is not possible to specify exact color ranges. Generally, the color should attempt to match those suggested for concrete unit pavers. However, other colors may be acceptable, such as if they matched the brick used for the building and the proposed color did not conflict with the approved streetscape colors.

### *Color*

The color range of paving materials for private property is broader than that for the public right-of-way. The colors specified below will provide private property owners with a palette of materials that will be compatible with the right-of-way improvements and will blend with most architectural styles and treatments. It has been necessary to specify color using a specific manufacturer's standard selection. However, an equivalent product can typically be obtained from other manufacturers.

For concrete unit pavers, the following colors are recommended (based on Claylite, Inc.'s standard color system):

- Arapahoe Blend
- Charcoal
- Charcoal, Arapahoe Blend and Brown mixtures

For Sandstone Paving:

- Red (muted shades)
- Buff

For thermal finish granite (colors based on the Cold Springs Granite Company's standard colors)

- Carnelian
- Sunset Beige

- Rockville Beige
- Rockville White

As the color of natural stone will vary with the source, samples from the quarry should be approved for each specific installation.

## Public Art

The contribution of public art to the life and vitality of the street is widely acknowledged. Art can provide focal points, create interest and impart a level of quality to a streetscape design. This understanding of the importance of public art for Vail was recognized with the establishment of the Art in Public Places Board (AIPP) and re-enforced by the positive comments received from the public during the design process for this plan.

Each individual piece of art should be carefully sited so that it compliments its setting. If possible, pieces should be commissioned and integrated into a particular space. The Master Plan has designated a number of potential locations for public art, however it is not intended that each one of these locations be a site for a piece. By identifying a range of sites, the most appropriate location can be selected as pieces become available. These sites will be further defined in a future Art Site Master Plan as proposed by the Art in Public Places Board.

Public art should not be limited to free-standing sculpture. Public art can take the form of fountains, murals, bas-relief, mosaics, etc. Art can, and should be incorporated into the design of common streetscape elements such as a bench, a planter wall, tree grate or even a manhole cover. Not only should public art take many forms but it should be of varying scales. Often the greatest pleasure can be derived from "discovering" smaller pieces of art in unusual or out-of-the-way locations.

As the AIPP program develops, the private sector may be called upon to provide artwork as a part of their development plan as they have been in the past for landscaping and other improvements.

## Site Furnishings

Benches, trash receptacles, bike racks, tree grates, etc. all contribute to a street's character. It is important that these elements be in keeping with the alpine character of Vail and be able to withstand the mountain environment.

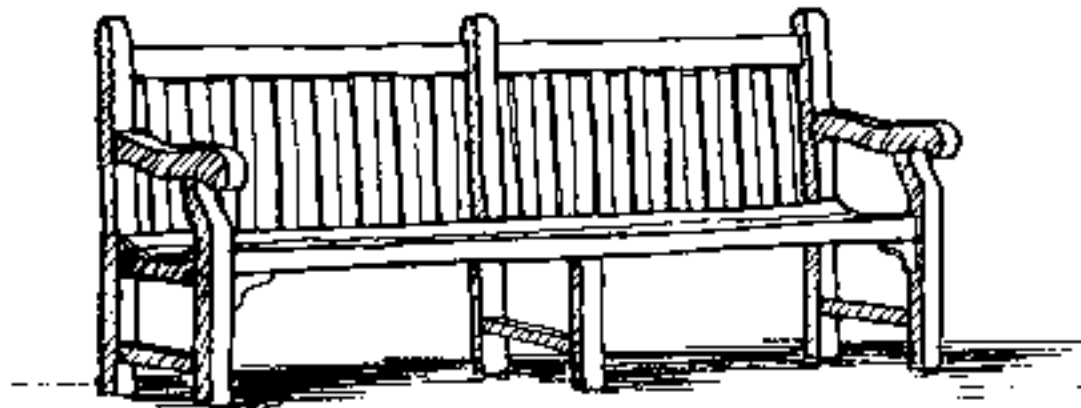
The following site furnishings have been selected because of their durability and character. Where possible, more than one style or manufacturer has been specified to allow for variety and to meet different conditions. The Town will want to consider reviewing new products on a regular basis.

Site furnishings also offer another opportunity for the introduction of public art. The cities of Seattle and Portland are well known for their creative approaches to designing public drinking fountains, manhole covers and light standards. This same approach could be adapted to Vail, and the result would be a fun, novel design that would enhance the streetscape.

No specific standards for site furnishings have been established for private properties in the study area, however it is recommended that private property owners utilize similar styles for site furnishings as established in this Plan for public properties. Coordination between the private property owner and the Town is beneficial to avoid duplication and to encourage the use of good quality site furnishings.

### BENCHES

Benches provide an excellent opportunity to incorporate ornamentation into the streetscape. They should be removable for possible storage during the winter months to allow for snowplowing. Wood benches are preferred as they are less impacted by temperature changes. The design should be distinctive and reflect craftsmanship. In natural settings, benches built out of stone, boulders or logs may provide unusual seating areas. Attention should be paid to how people will actually use the bench: Is the bench in the sun? Will people be comfortable sitting in groups or in twosomes on the bench? Is the bench comfortable? These criteria have led to the selection of the benches shown on the following page



**STREET BENCH**



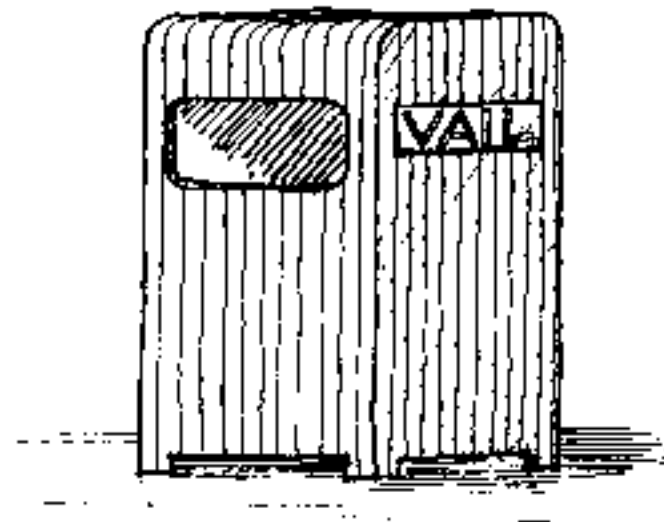
**LOG BENCH**

While the movable bench will be the primary bench used, it is possible to provide seating without disrupting snow plowing. One example is to attach benches to planter walls or buildings or to place benches around street trees. Standard

catalogue benches that have no relationship to Vail's streetscape character are not appropriate.

## TRASH RECEPTACLES

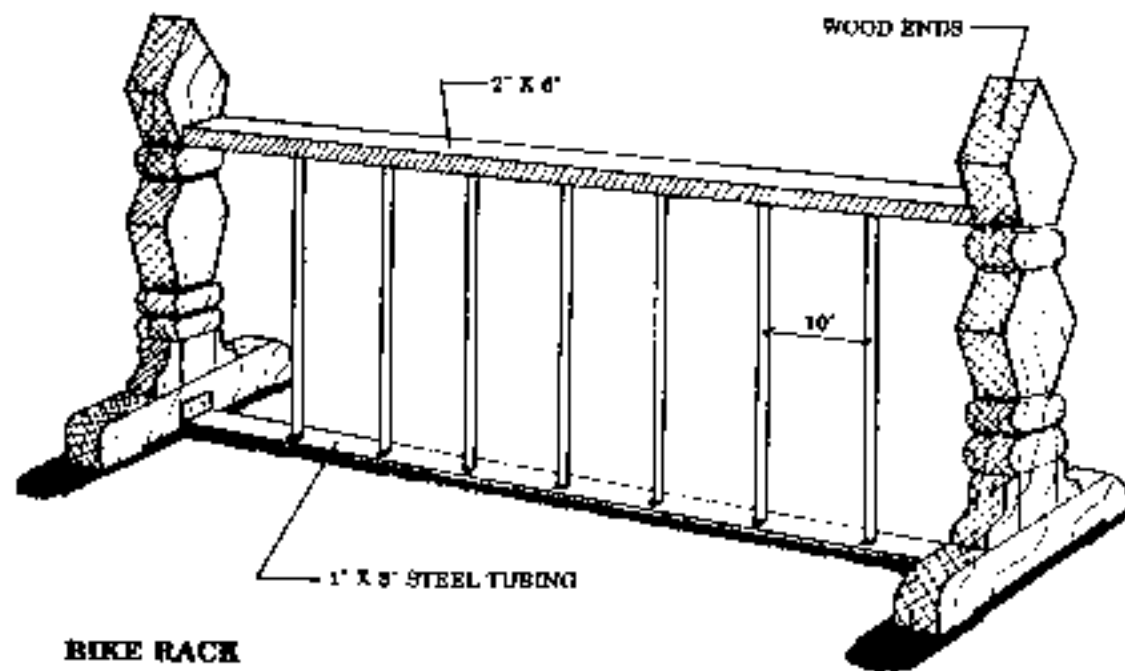
The selection of the bench types above have established a style and material that should be carried through to the other site furnishings. The trash receptacle shown below has been selected to coordinate with the wood bench. In addition, it has been chosen for cost, durability, ease of maintenance and because it has a closed top to keep out the snow. This design is simple and it could be manufactured by the Town Public Works staff. The totem detailing may be used on the trash receptacle.



TRASH RECEPTACLE

## BIKE RACKS

The bike rack shown below is based on a design developed with the Town staff. It is anticipated that it will be fabricated by the Public Works staff in lengths to fit each location. Design details similar to the totem details will be added to the bike racks.



## TREE GRATES AND GUARDS

Tree grates are recommended for street trees when they are used in an urban setting, such as the Village Core. A number of manufacturers produce this site amenity so the Town is not limited to the product shown below. The intent is to allow additional opportunities to bring a beautiful design accent into the streetscape.



**TREE GRATES**



**TREE GUARD**

## WALLS

Retaining walls and planter walls can make a significant contribution to the character of a streetscape. For the most part, Vail has seen great success in this area with the extensive use of stone-faced and boulder walls. The following guidelines are intended to build on that success.

- Walls should integrate into planters or be used for retaining earth. Freestanding walls are discouraged.
- The use of caps on walls should be evaluated on a site-by-site basis. If caps are to be used, then the cap should maintain a natural finish to the wall.

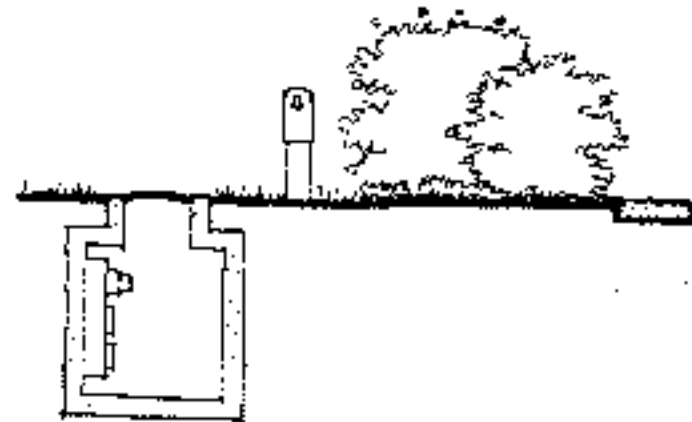


- Only durable materials should be used with a preference for locally obtained stone. Railroad tie walls are not recommended.
- Walls higher than 3 feet should be terraced.
- Large boulders should punctuate walls when possible to soften the linear appearance of a wall.
- Boulder walls are strongly encouraged, as this type of design is very compatible with Vail's natural setting.
- Walls should not be geometric, but should gently undulate.

## MISCELLANEOUS ELEMENTS

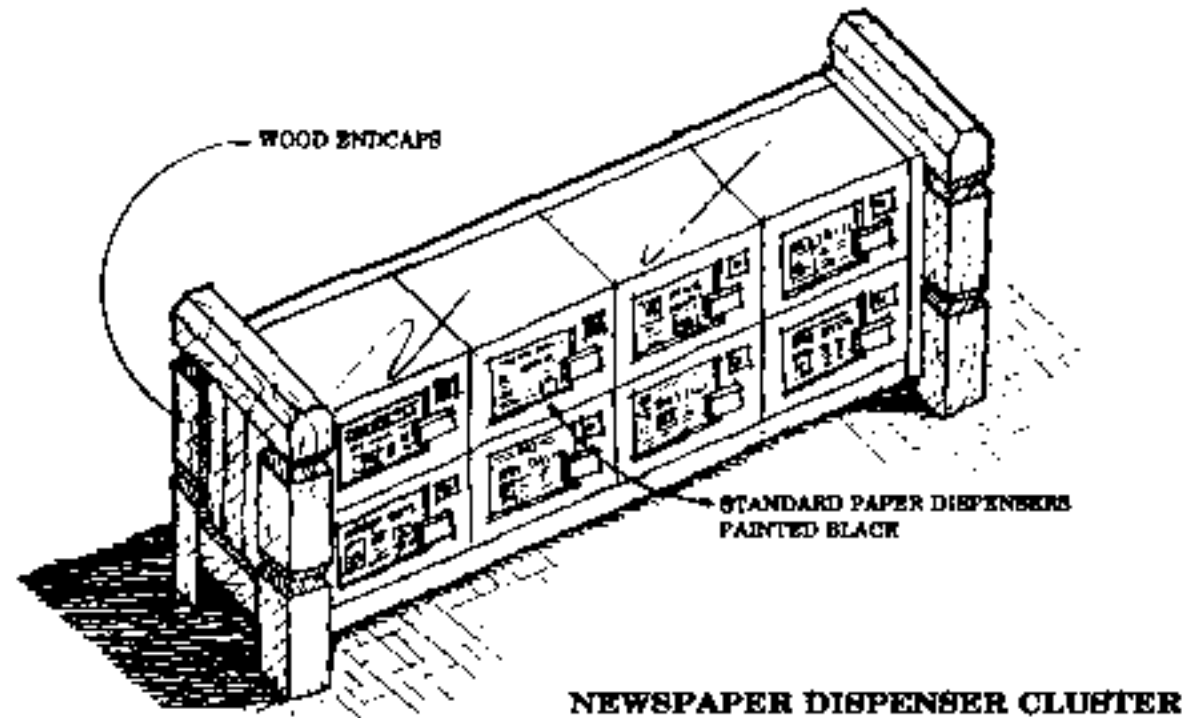
A number of miscellaneous site elements would typically be included in a streetscape plan such as decorative bollards, fencing, phone booths, bulletin boards, kiosks, etc. At this time, most of these elements have not been identified as being needed in the study area.

- Utility boxes and pedestals, when possible, should be placed in below grade vaults or, at a minimum, should be screened by landscaping.

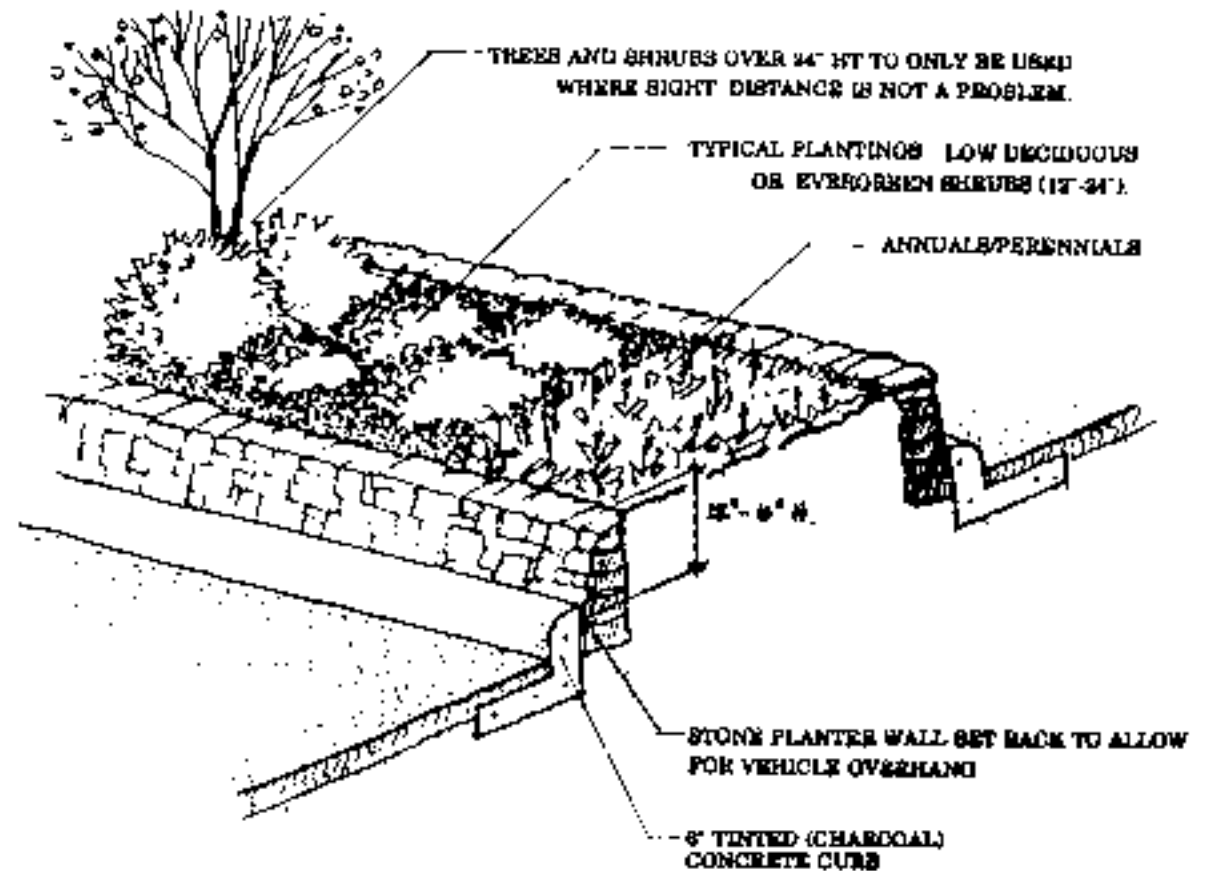


BELOW-GROUND OR SCREENED UTILITIES

- One element that has been requested is a clustered newspaper dispenser. The proposed design shown below is a continuation in detailing and style of the recommendations for the bike rack and trash receptacle selections. The actual newspaper dispensers are standard items that are purchased from private suppliers, painted and clustered into the desired configuration. Selected colors should blend with the surrounding area. The Town Public Works staff will be able to adapt the fixtures to the necessary specifications, using woodworking skills to bring a sense of craftsmanship to the enclosure.

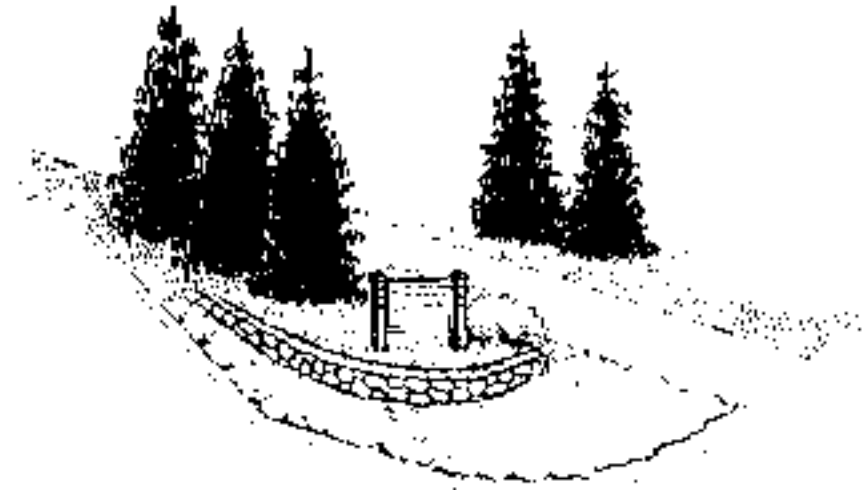


- Landscaped medians will be constructed on Vail Road and the South Frontage Road in the future. While more detailed design for these elements will be needed, the basic criteria and goals for medians are shown in the detail below.



**LANDSCAPE MEDIAN**

- In response to a concern that visitors are unaware that they are entering a pedestrian village when driving into Vail, an "entry feature" was suggested. This element could be used at key intersections to notify visitors that cars are restricted in some areas. One way this idea could be implemented is illustrated below.



PEDESTRIAN VILLAGE  
ENTRY FEATURE

# Lighting Master Plan

The need for a lighting master plan for Vail can quickly be demonstrated by walking around the Village at night. Some areas are well lit by colorful window displays, by building or landscape accent lighting and by street lights. All too often, however, the next street is dark and uninviting. This uneven distribution of lighting throughout the study area was frequently mentioned by the residents attending the public meetings, as one of their primary areas of concern regarding streetscape design. This awareness of the importance of effective lighting has resulted in the inclusion of a lighting component in the Streetscape Master Plan.

## LIGHTING INVENTORY

An inventory of nightlighting in the study area was made. This inventory not only looked at streetlights but also other light sources such as shop windows and accent lighting. Areas where the lighting was inadequate were identified. The coverage of the existing lighting fixtures is shown as gray circles in Figures 21 through 25. With this graphic representation of the existing lighting patterns, the areas that are poorly lit can be easily identified.

It should be noted that some areas, such as the Village parking structure portals and the top of Bridge street may actually be over-lit.

## THE LIGHTING CONCEPT

Too often lighting is not integrated into the overall streetscape environment. It is thought of only in terms of security and safety. In a town such as Vail, where providing a memorable night-time experience is critical, well designed street lighting is an essential part of a successful streetscape plan. Lighting should be incorporated into planters or landscaped areas whenever possible.

The goal of this section of the Master Plan is to develop a lighting plan that is appropriate for Vail. Because conditions vary from sub-area to sub-area, a uniform approach to lighting is not recommended. In the residential/lodging areas such as West Meadow Drive or Vail Valley Drive, a typical street lighting scheme will suffice with minimal accent lights. Screen landscaping should be located between residential dwellings and the lighting along public rights-of-way. However, in the

commercial areas such as East Meadow Drive and the Village Core, simply adding a streetlight on every corner will not work. The lighting concept in these areas must take into account all of the potential light sources. The lighting plan will be more effective if the illumination originates from a variety of sources. Lighting designs should always consider the winter snow depths and snow storage areas.

The proposed approaches to lighting are as follows:

- East Lionshead Circle and the Library/Ice Arena Plaza (Figure 21)

The lighting scheme for East Lionshead Circle will be essentially unchanged except for upgrading the existing "Lionshead" fixtures to the "Village" fixture. Additional fixtures are needed in the bus turnaround at the west end of the area.

The connection to the Library/Ice Arena Plaza, the "Chute," is in need of additional lighting. This area could be illuminated either by using bollard lighting or the "Village" fixture.

Additional lighting is also needed for the Library/Ice Arena Plaza. This is the one area outside of the Village Core where a mixture of lighting sources should be used, including the "Village" fixture, bollard lights, landscape accent lights, and wall lights. The "Village" fixture should replace the "Lionshead" fixtures now in use in this area.

- West Meadow Drive (Figure 22)

The existing "Town and Country" fixtures are appropriate in this area for the short term. As a long term goal, however, these lights should be replaced by the "Village" fixture as this will provide a consistent treatment. Since the "Village" fixture is shorter, more lights will be required to achieve the same lighting level. Additional lights also are needed on the north side of the street.

- East Meadow Drive (Figures 23 & 24)

The lighting along East Meadow Drive certainly does not reflect its position as one of Vail's primary retail commercial areas. The lighting design is not only inappropriate but along the west half of the area, the light levels are also inadequate. There is essentially no lighting along the south side of the street. The orange tinted, cube fixtures found along the west half of West Meadow Drive should be replaced by the "Village" fixture. The intersection of Willow Bridge Road and East Meadow Drive is also seriously underlit. Existing pole and building mounted accent lights that shine directly down on pedestrians should also be replaced.

A framework of streetlighting using the "Village" fixture is needed throughout the corridor. For areas where there are commercial uses fronting on the street, a mixture of light sources should be encouraged. Window displays should also be included as a part of the overall lighting scheme. Private property owners should be encouraged to install subtle lighting for landscaped areas along with bollard lights and building illumination. These supplemental light sources should be used to round-out the framework established by the Town's right-of-way lighting.

The intersection of Willow Bridge Road and East Meadow Drive should have a higher lighting level than other sections of the street. On the east half of Meadow Drive, additional lighting is needed, especially along the area adjacent to the Village parking structure.

- The Village Core (Figure 25)

The nightlighting for the Village Core should be varied and carefully planned. By far, the best approach is have one integrated lighting plan.

While some areas such as the Gore Creek Promenade and upper Bridge Street are adequately lit, other areas such as Gore Creek Drive along the Lodge at Vail, need additional lighting. It is recommended that the Town provide a basic framework of "Village" fixtures and that the private property owners be encouraged to supplement the Town's lighting with their own accent lighting. In time, this system would achieve the proper level of lighting throughout the Village Core and still provide the range of varied light sources that provide the best lighting scheme. Lighting plans

for each proposed upgrade will need to be carefully evaluated and compared to the existing conditions.

Figure 25 shows one possible approach to creating the appropriate level of lighting for the Village Core area using a variety of light sources.

- East Village (Figure 18)

The lighting needs for the East Village are similar to West Meadow Drive. A good framework of "Town and Country" lights is already in place and it will only be necessary to add lights where there are gaps in the coverage. For this sub-area the "Town and Country" fixture will be appropriate for the foreseeable future.



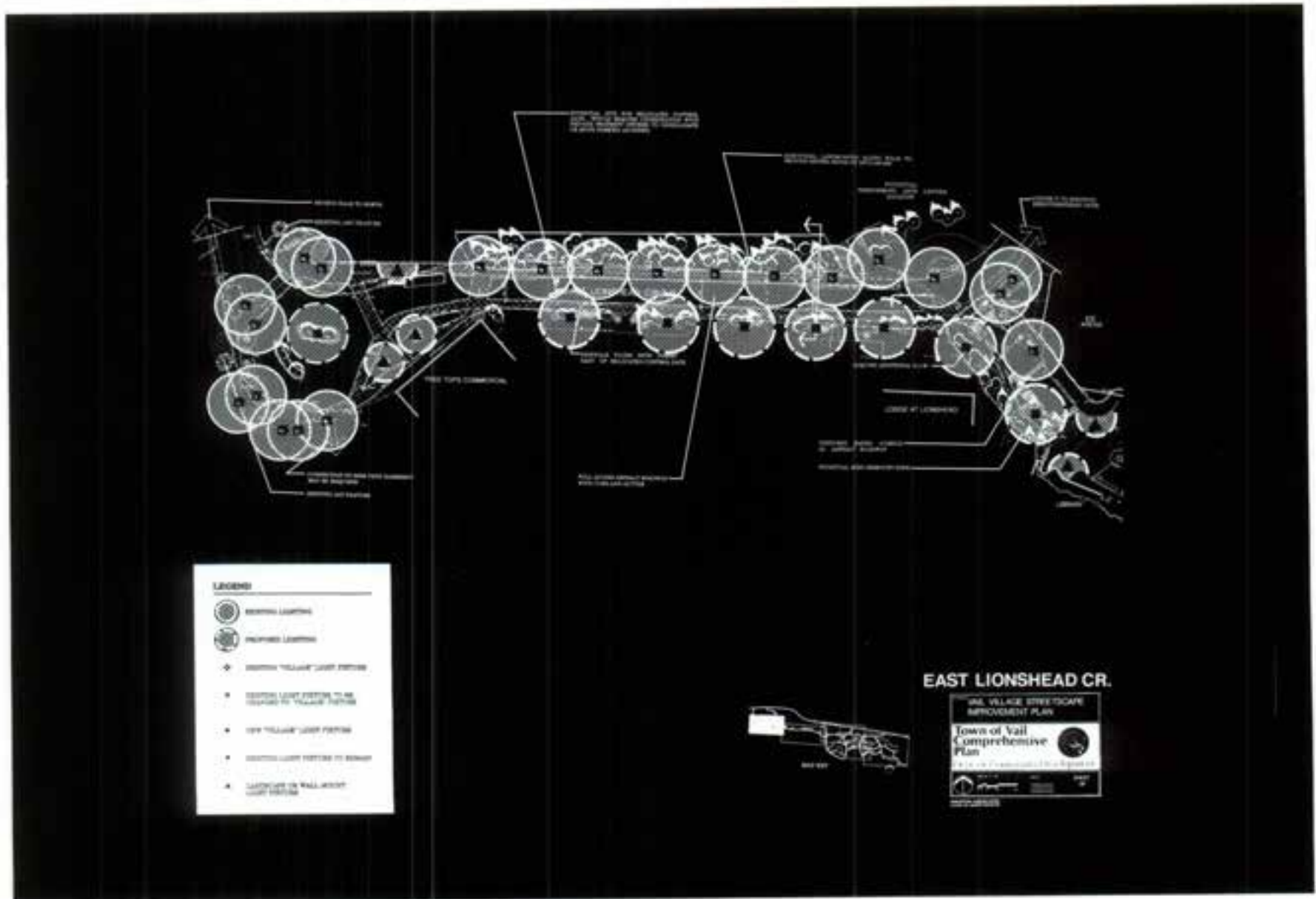


Figure 21

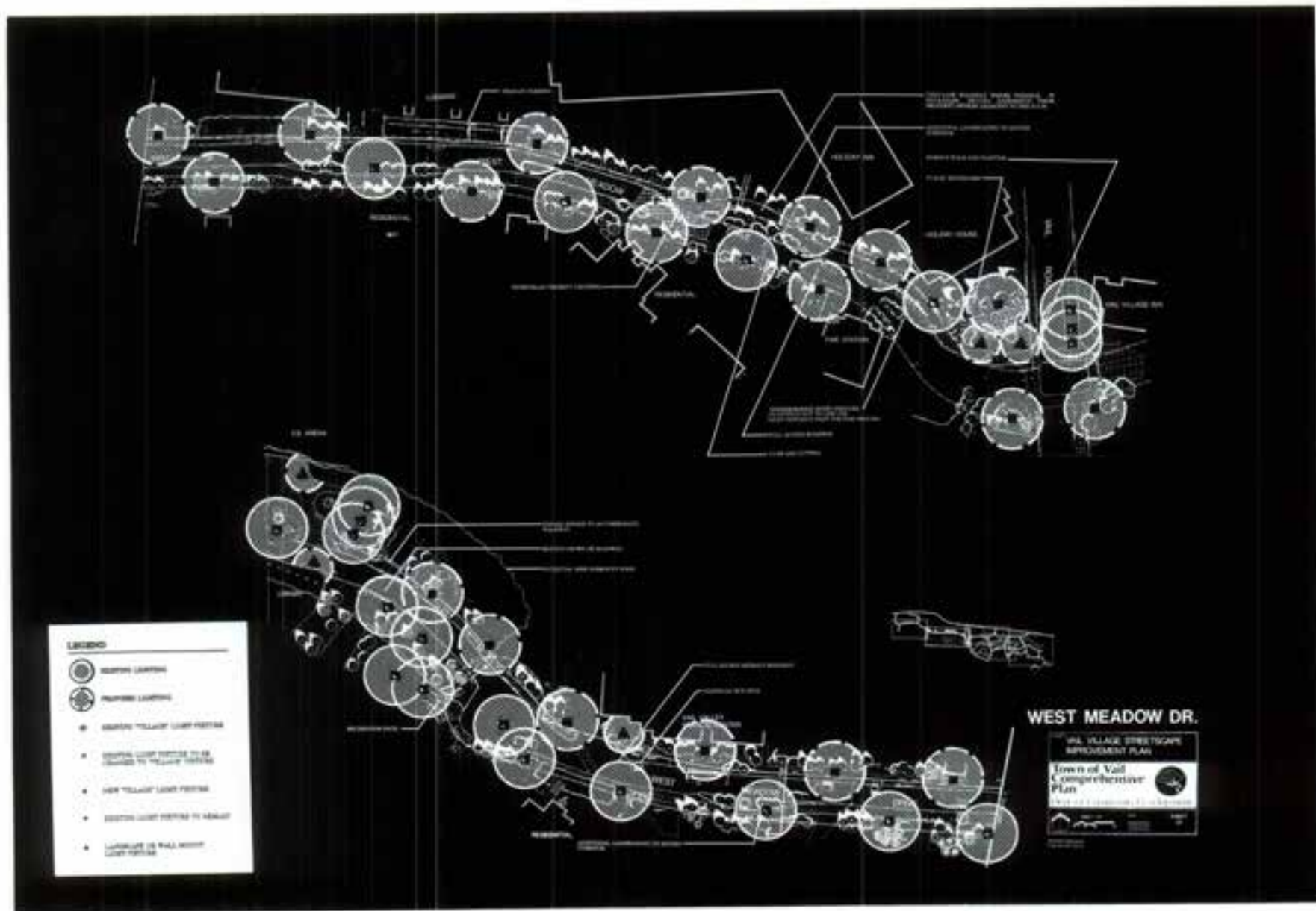


Figure 22

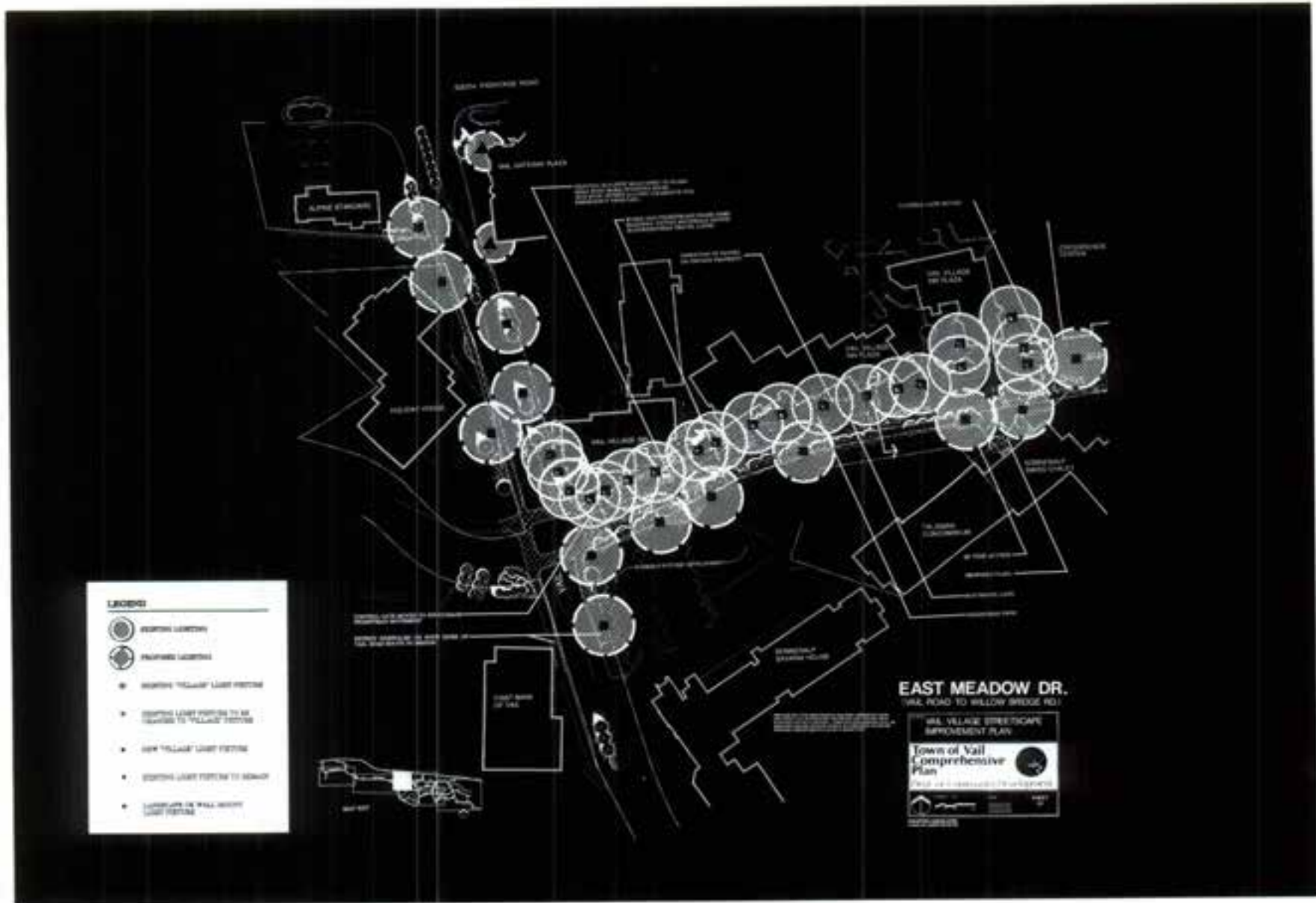


Figure 23

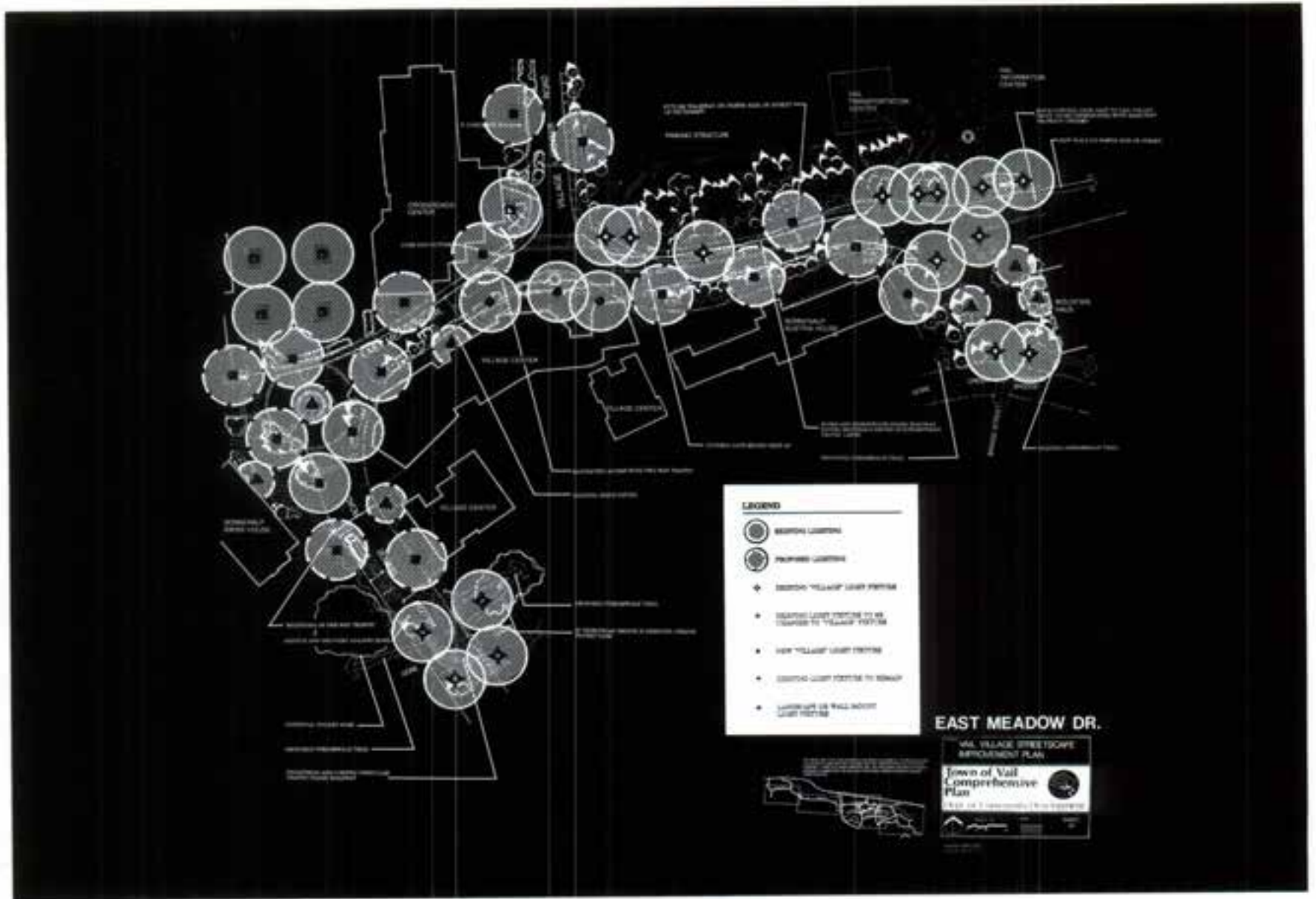


Figure 24



Figure 25

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## IMPLEMENTATION AND MAINTENANCE

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This section will address the impacts of the recommendations of the Master Plan and specifically the impacts on maintenance. Providing Vail with a quality streetscape will take time and money, and it is clear that it will not be possible to maintain the new streetscape improvements with the Town's present maintenance budget.

### Implementation

Due to the scale of the Streetscape Master Plan, the proposed improvements will need to be implemented over a period of years. The Master Plan has been conceived with phased construction in mind and this will allow the Town flexibility in planning which improvements are to be constructed first. The Plan also allows for adjustments in priorities should implementation opportunities arise.

When streetscape improvements are to be constructed over a broad area, and over a number of years, prioritizing those improvements can be a difficult problem. Each of the following factors must be taken into account when determining the final order of construction for the Streetscape Master Plan:

#### *Safety*

Maintaining a safe pedestrian environment is an important factor in determining priorities. When the recommendations of the Master Plan are implemented, the following pedestrian areas will realize the greatest increases in safety and comfort:

- The Library "Chute" between the Library/Ice Arena Plaza and East Lionshead Circle
- West Meadow Drive
- East Meadow Drive
- Vail Valley Drive

### *Below Ground Utilities*

Construction of right-of-way improvements and sub-surface utility improvements are disruptive to the streetscape environment. Disruption would be lessened if all streetscape and utility upgrade projects were coordinated together. It is recommended that the Town and the utility companies work together to complete any underground work prior to constructing streetscape improvements. This level of cooperation is even more critical since Vail's construction season is very short. For example, the Upper Eagle Valley Water and Sanitation District has plans to upgrade water service soon, primarily in the Village Core. The Town of Vail also needs to upgrade or install storm sewer lines in the same area. The timing of these projects will have a substantial effect on the timing of the streetscape improvements. The Town should plan the streetscape improvements and storm sewer work to coincide with the utility work. The impact of construction will be lessened as will the cost of both utility and streetscape work.

### *Private Development*

As private property in Vail is being redeveloped and/or upgraded, an opportunity exists for many of the proposed Master Plan improvements to be done in conjunction with or entirely by private property owners. For example, the improvements in the Gore Creek Promenade were funded in this manner. There is no way to predict which property will be redeveloped, therefore, each application for redevelopment will need to be reviewed to determine what streetscape improvements can be completed as part of the private construction.

### *Achieving the Greatest Impact*

Elements of the Streetscape Plan that are more visible and create the greatest impact should have the highest priority. Some elements that fit in this category are:

- The focal point areas:
  - The Children's Fountain
  - Seibert Circle
  - The intersection of Bridge Street and Gore Creek Drive
  - The intersection of Willow Bridge Road and East Meadow Drive
  - The Ski Museum pocket park
  - Slifer Square.

These key areas have been designed in the Master Plan to stand as independent units and therefore can be improved in advance of the remaining streetscape work.

These feature areas should be used to generate public interest and, as people see the results of the streetscape improvements, support for the streetscape program will grow.

- Landscaping. The Town should allocate some money each year to add landscaping in the public right-of-way, even if it means planting smaller trees and shrubs.
- Lighting. The Town should allocate some money each year to add lighting.
- Public art. The Town should allocate some money each year to add public art.

While it would be possible to list a recommended order for streetscape improvements, that is probably not the best approach for Vail given the rapid rate of change. The Town staff, the Design Review Board, the Planning and Environmental Commission and the Town Council will need to set achievable short term goals and then be flexible enough to react to new opportunities or utility improvement schedules.

## Funding

Funding the proposed improvements is another key element in implementing the Master Plan. Special improvement districts and bond initiatives are additional funding mechanisms available to the Town. The other possible funding sources are the Town's Capital Improvements Program, proceeds from the Real Estate Transfer Tax, and construction as part of private development. These are all viable funding sources, but can result in piecemeal construction. Given that the Plan has anticipated a phased development schedule, however, this will not be a problem. However, even phased implementation creates problems. For instance, it can be difficult to change grades or surface drainage patterns on a street without paving the whole street. Careful planning and design will be needed to avoid problems such as this.



## Maintenance

Proper maintenance is the final key to a successful Streetscape Master Plan. Visitors expect resort areas to be maintained at a higher level than the average town. Adequate snow removal measures are imperative in areas such as West Meadow Drive, Vail Valley Drive and East Lionshead Circle, where new pedestrian ways are proposed. Pedestrians will continue to walk in the street when walkways are not snowplowed or only partially plowed. It is essential that the Town consider adjusting maintenance staffing and funding as each new pedestrian way is constructed to ensure that the pedestrian areas are utilized as planned.

Areas of concern regarding maintenance that must be considered with the new streetscape plan are:

### *Loss of Snow Storage Areas*

As Vail continues to develop and/or as more landscaping is added, areas which are currently available for snow storage will be reduced. As this trend continues, areas in the Village Core and along West Meadow Drive, where snow has been stored temporarily for later removal, will have to be plowed and snow removed immediately. This actually is preferred from an appearance standpoint; but the cost of snow removal will increase. The Town should consider purchasing equipment such as "paddle-wheel" style loaders for quicker snow removal. One benefit of the Streetscape Plan is that planters added in the Village Core will create new snow storage areas.

### *Plowing Walkways*

Plowing pedestrian walkways requires smaller, lighter equipment than is necessary for streets. The Town will have to consider purchasing new equipment or adapt the existing equipment to remove snow from the proposed walkways on West Meadow Drive, Vail Valley Drive and East Lionshead Circle.

Another option would be to strictly enforce the snow shoveling ordinance which would require property owners to shovel and remove the snow from walks adjacent to their property. This has not been effective in other cities since some property owners often cannot shovel until late in the day and others will ignore it altogether. Even a short segment of unshoveled walkway will encourage the pedestrian to use the street.